

**GLEN LAKE'S COMMONS  
SIGNALIZED INTERSECTION CONSTRUCTION AGREEMENT**

THIS AGREEMENT (this "Agreement") is made and entered into this 8th day of April, 2025, by and between the Hernando County, a political subdivision of the State of Florida ("County") and Glen Lake's Commons, LLC ("GLC"). The County and GLC shall each be referred to herein as a "Party" and shall be collectively referred to herein as the "Parties."

**RECITALS**

WHEREAS GLC is the developer of the approximate 21.38-acre property described as Glen Lake's Commons commercial development ("GLC Project", as further described in Exhibit "A"); and

WHEREAS, the County has planned a to-be-named county frontage road connecting from the south to the north Glen Lakes Boulevard and Bourassa Boulevard along the west side of U.S. Highway 19, as approved by the Hernando County engineer; and

WHEREAS, GLC desires to cooperatively to assist the County in constructing the Signalized Intersection Project (as defined below) as specifically provided for in this Agreement; and

WHEREAS, the Parties desire to enter into this Agreement in order to provide for the rights and responsibilities of the Parties regarding the design, permitting, construction and funding of the Signalized Intersection Project.

NOW, THEREFORE, in consideration of the covenants hereinafter contained, and other good and valuable consideration, the receipt of which is hereby acknowledged, the Parties agree as follows:

**AGREEMENT**

1. **Recitals.** The above referenced recitals are true and correct and are incorporated herein by reference.
2. **Signalized Intersection Relocation Project Plans and Specifications.** The Parties plan for a signalized intersection as depicted in Exhibit "B". The Parties agree to the plans and design specifications for the Signalized Intersection Project which are contained in the "Construction Plans for the U.S. Highway 19 and Glen Lakes Blvd Intersection Improvements" prepared by Lincks & Associates, Inc. ("Lincks") and attached hereto as **Composite Exhibit "C" ("Plans and Specifications")**. Any material modifications to the Plans and Specifications shall require the approval of the Parties, which approval may be confirmed in a separate written instrument, including electronic format, by each Party. The County agrees that any approval for such modifications may be administratively approved and delivered by the Director of Public Works/County Engineer.
3. **The County's Role – Project Manager.** The Parties agree that the County shall be the project manager responsible for the design, permitting, construction, and funding of the Signalized Intersection Relocation Project as provided in this Agreement. Within ten (10) business days of the Effective Date of this Agreement, the County will coordinate with the Florida Department of Transportation ("FDOT") to assume the role of applicant and permittee with respect to the required FDOT application for construction, and take all necessary actions to obtain FDOT's approval of the Plans and Specifications, together with any other required permits to effectuate the construction

of the Signalized Intersection Project including the finalization and execution by the County of the FDOT Construction Agreement (collectively, "**Government Authorizations**"). The County shall use reasonable efforts to obtain all required Governmental Authorizations for the Signalized Intersection Project within ninety (90) calendar days of the Effective Date of this Agreement. Any modifications to the Plans and Specifications must be approved by the Parties as set forth in Paragraph 2 above.

4. **Project General Contractor, Engineer, and Bidding Process for Final Section of Construction Contractor.** The Parties agree that GLC shall be the independent contractor responsible for the management of the construction of the Signalized Intersection Project, and Lincks shall continue to serve as the engineer. GLC agrees to solicit sealed alternate competitive bids to determine the Final Project Costs (defined below) for the Signalized Intersection Project and select the construction contractor. The final selection of the construction contractor shall be based upon the lowest reasonable and responsive alternate competitive bid. GLC shall reasonably attempt to obtain at least three (3) competitive bids, if possibly. GLC agrees to otherwise substantially comply with the County's bid process. Each bid shall clearly indicate the cost of each component of the Signalized Intersection Project. GLC also agrees County personnel may, but are not obligated to, be present when the sealed bids are opened. GLC further agrees to allow the County to concurrently approve and accept the most reasonable and responsive low bid, and to approve contract documents and related payment terms prepared by GLC. The bid award for the Signalized Intersection Project will require Hernando County Commission approval. The County shall be responsible for entering into all agreements with the construction contractor, Lincks and other professionals/consultants performing work on the Signalized Intersection Project and, subject to Paragraph 6 below, shall be the responsible party for paying all contractors, professionals and consultants directly.
5. **Estimated and Final Project Costs.** Attached as **Exhibit "D"** is an estimate in the amount of \$1,325,058.00 from Lincks which represents the design, permitting and construction costs for the Signalized Intersection Project ("**Estimated Project Costs**"). The final Signalized Intersection Project costs shall be the total costs for the Signalized Intersection Project, including without limitation design, permitting and construction costs, upon the final completion of the Signalized Intersection Project on the date that the County obtains approval of the final Signalized Intersection Project as-built from all applicable governmental permitting entities, including FDOT and the County ("**Final Project Costs**").
6. **Cost-Sharing.** Attached as **Exhibit "E"** is the transportation analysis prepared by Lincks dated January 29, 2025 ("**Transportation Analysis**") which provides a proportionate share allocation for the Signalized Intersection Project. The Transportation Analysis assigns Seventy-Three and Sixty-Six Hundredths Percent (73.66%) of the Final Project Costs to the County which equates to \$976,037.72, based on the Estimated Project Costs, and Twenty-Six and Thirty-Four Hundredths Percent (26.34%) of the Final Project Costs to GLC, which equates to \$349,020.28, based on the Estimated Project Costs (the "**GLC Estimated Contribution**"). The Parties agree that the Final Project Costs shall be paid as follows:
  - a. Following the Effective Date and during the construction of the Signalized Intersection Project, GLC shall submit paid invoices for any and all out-of-pocket expenses related to the Signalized Intersection Project, including without limitation fees and costs paid to Lincks and other approved "soft costs" set forth in Exhibit "D" (the "**GLC Expenses**"). The GLC Expenses shall be credited toward the GLC Estimated Contribution.

- b. Subject to the County's determination in accordance with Hernando County Code of Ordinances Section 23-144 that the Signalized Intersection Project is impact fee creditable, the Parties agree that any and all impact fees payable to the County by purchasers or tenants of parcels within the GLC Project which become due prior to completion of the Signalized Intersection Project (collectively, "GLC Credits") shall be credited toward the GLC Estimated Contribution.
  - c. Upon GLC's payment of GLC Expenses and/or application of GLC Credits in a cumulative sum equal to the GLC Estimated Contribution, GLC shall have no further monetary obligations with respect to the Signalized Intersection Project. The County shall be solely responsible for paying Final Project Costs that exceed the GLC Estimated Contribution.
  - d. No later than one hundred twenty (120) calendar days after completion of the construction of the Signalized Intersection Project, the County shall provide GLC with itemized documentation of the Final Construction Costs paid by the County and with respect to the entirety of the Signalized Intersection Project.
  - e. In the event that the Final Project Costs are less than the Estimated Project Costs, the County shall refund GLC's overpaid portion of the GLC Estimated Contribution no later than one hundred eighty (180) calendar days after completion of the Signalized Intersection Project. The Parties hereby agree that such refund shall be in the form of transportation impact fee credits, if available, or, if not available, in the form of immediately available funds.
7. **Pre-Construction Conference.** GLC shall hold a pre-construction conference for the Signalized Intersection Project and notify the County so the County may attend and participate, at which time GLC shall provide the County with a proposed notice to contractor(s) to commence ordering materials and prepare shop drawings within thirty (30) days.
8. **Inspection.** GLC agrees to permit the County inspector(s) to be always present during construction of the Signalized Intersection Project, subject to any and all applicable workplace safety regulations.
9. **Remedies.** In the event of a breach of any Party's obligations under this Agreement, a cause of action shall immediately accrue to the non-breaching Party, and such non-breaching Party shall be entitled to pursue all remedies in law and equity.
10. **Notices.** All notices which are required or permitted hereunder must be in writing and shall be deemed to have been given, delivered or made, as the case may be (notwithstanding lack of actual receipt by the addressee) (i) upon hand delivery; (ii) one (1) business day after having been deposited with an expedited, overnight courier service (such as by way of example but not limitation, U.S. Express Mail, Federal Express, or UPS), or (iii) upon delivery of a facsimile transmission with electronic delivery verification to the following addresses or numbers:

If to GLC:                      Glen Lake's Commons, LLC  
2400 Tamiami Trail North  
Suite 400  
Naples, Florida 34103  
E-mail: bob@sitetokey.com; clay@wbdcinc.com

Courtesy copy to: Spencer Fane LLP  
201 North Franklin Street  
Suite 2150  
Tampa, Florida 33602  
Attn: Derek Larsen-Chaney  
Fax: (813) 405-8904  
E-mail: dlarsenchaney@spencerfane.com

If to the County: Hernando County  
15470 Flight Path Drive  
Brooksville, FL 34604  
Attn: County Administrator  
Fax (352) 754-4477  
Email: administration@co.hernando.fl.us

Courtesy copy to: Hernando County Attorney's Office  
20 N. Main St., Ste. 462  
Brooksville, FL 34601  
Attn: Garth Coller, Esq.  
Fax: (352) 754-4001  
E-mail: CAO@hernandocounty.us

The failure by any party to deliver a courtesy copy as referenced above shall not constitute a default under the terms of this Agreement nor shall it create a defect in any notice which is otherwise properly given. Furthermore, it is agreed that, if any party hereto is represented by legal counsel, such legal counsel is authorized to deliver written notice directly to the other party on behalf of his or her client, and the same shall be deemed proper notice hereunder if delivered in the manner hereinabove specified. Any party hereto may, at any time by giving ten (10) business days written notice to the other party hereto, designate any other address in substitution of the foregoing address to which such notice shall be given and other parties to whom copies of all notices hereunder shall be sent.

11. **Miscellaneous.**

- a. **Participation.** Each of the Parties have participated fully in the negotiation and preparation of this Agreement with full benefit of counsel. Accordingly, this Agreement shall not be more strictly construed against any of the Parties, and shall be interpreted as if the Parties hereto jointly prepared it.
- b. **Cooperation.** The Parties shall execute in good faith such other and further documents as may be required to effectuate the terms of this Agreement.
- c. **Force Majeure.** If the performance of any obligation under this Agreement is prevented, restricted, or interfered with by reason of war, revolution, civil commotion, acts of public enemies, pandemic, epidemic, blockade, embargo, strikes, acts of God, floods, tropical storms or hurricanes, then the affected party shall, upon giving prior written notice to the other party, be excused from such performance to the extent of such prevention, restriction, or interference, provided that the affected party shall use commercially reasonable efforts to avoid or remove such causes of nonperformance, and shall continue performance hereunder with reasonable dispatch whenever such causes are removed.



- d. **Attorneys' Fees/Venue.** In the event of any dispute, litigation, or other proceeding between the Parties arising out of this Agreement, to enforce any provision of this Agreement, or any right of the Parties hereunder, each party to such dispute, litigation, or other proceeding shall pay its own attorney fees, costs and expenses incurred in court, at trial, on appeal, and in any other proceeding irrespective of whether a party prevails in such litigation or proceeding. In the event of litigation, venue shall be in Hernando County, Florida. The provisions of this Section shall survive termination of this Agreement.
- e. **Relationship of the Parties; No Third-Party Beneficiaries.** Nothing contained in this Agreement is intended to, or shall, or shall be deemed to, create a joint venture or partnership of any kind between the Parties hereto. Nothing contained in this Agreement is intended to create any rights in third parties.
- f. **Waiver of Jury Trial.** THE PARTIES JOINTLY AND SEVERALLY, HEREBY KNOWINGLY, VOLUNTARILY, AND INTENTIONALLY WAIVE THE RIGHT THEY MAY HAVE TO A TRIAL BY JURY IN RESPECT OF ANY LITIGATION BASED HEREON, OR ARISING OUT OF, UNDER OR IN CONNECTION WITH THIS AGREEMENT AND ANY AGREEMENT CONTEMPLATED TO BE EXECUTED IN CONJUNCTION HERewith, OR ANY COURSE OF CONDUCT, COURSE OF DEALING, STATEMENTS, WHETHER VERBAL OR WRITTEN, OR ACTIONS OF EITHER PARTY.
- g. **Severability.** In the event any one or more provisions contained in this Agreement shall for any reason be held invalid, illegal, or unenforceable in any respect, such invalidity, illegality, or unenforceability shall not affect any other provision hereof and this Agreement shall be construed as if such invalid, illegal, or unenforceable provision had not been contained herein.
- h. **Assignment.** Assignment of this Agreement by any Party must be approved in writing by the other Parties, which approval shall not be unreasonably withheld.
- i. **Binding Effect.** This Agreement shall be binding upon and shall inure to the benefit of the successors and assigns of the respective Parties.
- j. **Authority.** If applicable, the entity officer or manager executing this Agreement certifies by acknowledgment of the signature below that s/he has been properly authorized to enter into this Agreement on behalf of, and binding with respect to, such entity.
- k. **Counterparts.** This Agreement may be executed in any number of counterparts, each of which when so executed and delivered shall be an original, but such counterparts shall together constitute but one and the same instrument.
- l. **Amendment.** This Agreement is the complete agreement of the Parties and may not be changed, orally, but only by instrument in writing signed by the Parties.
- m. **Headings.** Titles and captions to paragraphs and sections are inserted for convenience only, and in no way define, limit, extend or describe the scope or intent of this Agreement or the paragraphs, sections or provisions herein.

- n. **No Waiver.** Failure of any Party to exercise any right or power given hereunder, or to insist upon compliance by another Party with its obligations set forth herein, shall not constitute a waiver of any Party's right to demand strict compliance with the terms and provisions of this Agreement. Parties further agree that the waiver of a Party's breach or threatened breach of any obligations under this Agreement shall not be construed as a waiver of any subsequent breach by that Party.
- o. **Business Day.** Business day means any day other than Saturday or Sunday or a legal holiday.
- p. **Effective Date.** The "Effective Date" of this Agreement shall be the date that the last of the Parties execute this Agreement.

*(Remainder of page intentionally left blank. Signature page(s) to follow.)*

IN WITNESS WHEREOF, the Parties hereto have set their hands and seals as of the dates set forth below.

HERNANDO COUNTY

ATTEST:

*ja* Hindi Prouse, Deputy Clerk  
Douglas Chorvat Jr.  
Clerk of the Circuit Court



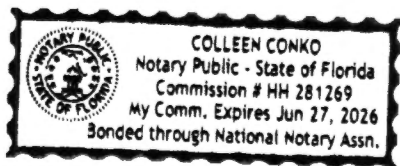
Brian Hawkins  
Brian Hawkins  
Hernando County, Board Chairman

Date: 4-8-2025

STATE OF FLORIDA  
COUNTY OF HERNANDO

The foregoing instrument was acknowledged before me by means of ☒ physical presence or ☐ online notarization, this 8th day of April, 2025, by Brian Hawkins as Chairman of the Hernando County Board of County Commissioners, who is personally known to me or who has produced \_\_\_\_\_ as identification.

[Notary Seal]



Colleen Conko  
Notary Public  
Colleen Conko  
Name printed

My Commission Expires: 06-27-2026

FOR THE USE AND RELIANCE OF  
HERNANDO COUNTY ONLY.  
APPROVED AS TO FORM  
AND LEGAL SUFFICIENCY.

Victoria Anderson  
County Attorney's Office

WITNESSES:

Aliciana D. Cantrell  
Print Name: Aliciana D. Cantrell  
Address: 11173 Riley Pines Circle  
Gibsonton, FL 33534

Elizabeth A. Coutu  
Print Name: Elizabeth A. Coutu  
Address: 7203 Meeting House Lane  
Apollo Beach, FL 33572

GLEN LAKE'S COMMONS, LLC

By: [Signature]  
Name: ROBERT GOMEZ JR.  
Title: MANAGING PARTNER  
Date: 3/26/2025

STATE OF Florida  
COUNTY OF Hillsborough

The foregoing instrument was acknowledged before me by means of ☒ physical presence or ☐ online notarization, this 26th day of March, 2025, by Robert Gomez, Jr. as Managing Partner of Glen Lake's Commons, LLC, a Florida limited liability company, who is personally known to me or who has produced \_\_\_\_\_ as identification.

[Notary Seal]



[Signature]  
Notary Public

Lori N. Harris  
Name printed

My Commission Expires: June 14, 2025

**Exhibit "A"**

**LEGAL DESCRIPTION FOR GLEN LAKE'S COMMONS**

THAT PART OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, TOWNSHIP 22 SOUTH, RANGE 17 EAST, HERNANDO COUNTY, FLORIDA, LYING WEST OF U.S. HIGHWAY #19.

BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHWEST CORNER OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, TOWNSHIP 22 SOUTH, RANGE 17 EAST, HERNANDO COUNTY, FLORIDA; THENCE ALONG THE WEST LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, RUN N01°18'06"E, 1330.05 FEET TO THE NORTH LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SAID SECTION 13; THENCE N50°41'32"W, 57.35 FEET TO A NON-TANGENT CURVE TO THE RIGHT AND THE EAST RIGHT-OF-WAY LINE OF NEW ORLEANS DRIVE, SAID CURVE BEING CONCAVE TO THE SOUTHEAST, HAVING A RADIUS OF 175.00 FEET, A CENTRAL ANGLE OF 31°34'08", AND A CHORD BEARING AND DISTANCE OF N23°39'40"E, 95.21 FEET; THENCE ALONG THE ARC OF SAID CURVE 96.42; THENCE N39°20'26"E, 238.56 FEET; THENCE LEAVING SAID EAST RIGHT-OF-WAY LINE, RUN S50°40'56"E, 63.11 FEET; THENCE S47°48'06"E, 62.58 FEET; THENCE S24°09'32"E, 93.48 FEET; THENCE S53°13'38"E, 43.57 FEET; THENCE S80°46'20"E, 82.26 FEET TO A NON-TANGENT CURVE TO THE LEFT, AND THE WEST RIGHT-OF-WAY LINE OF OUTER BANKS DRIVE, SAID CURVE BEING CONCAVE TO THE EAST, HAVING A RADIUS OF 353.95 FEET, A CENTRAL ANGLE OF 7°52'58", AND A CHORD BEARING AND DISTANCE OF S05°18'47"W, 48.66 FEET; THENCE ALONG THE ARC OF SAID CURVE 48.70 FEET; THENCE S01°10'27"W, 62.01 FEET TO THE NORTH LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SAID SECTION 13; THENCE ALONG SAID NORTH LINE, RUN S88°40'06"E, 49.97 FEET TO THE EAST RIGHT-OF-WAY LINE OF OUTER BANKS DRIVE; THENCE CONTINUE ALONG SAID NORTH LINE, RUN S88°42'23"E, 350.66 FEET TO THE WEST RIGHT-OF-WAY LINE OF U.S. HIGHWAY #19; THENCE ALONG SAID WEST LINE, RUN S14°08'08"W, 1365.17 FEET; THENCE LEAVING SAID WEST LINE ALONG THE SOUTH LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, RUN N88°33'43"W, 486.32 FEET TO THE POINT OF BEGINNING.

CONTAINING 21.38 ACRES MORE OR LESS.



**Exhibit “B”**

**Depiction of Signalized Intersection**



ACCESS EXHIBIT

GLEN LAKES COMMONS

**Coastal**  
 ENGINEERING ASSOCIATES, INC.  
 1000 Cambridge Road, Suite 100  
 Raleigh, NC 27601  
 (919) 796-8423 • Fax: (919) 796-8208

REUSE OF DOCUMENT  
 THIS DOCUMENT, COMPOSED OF THE  
 INCORPORATED, SEALS AND DESIGNS  
 OF THE PROFESSIONAL ENGINEER,  
 PROFESSIONAL SEAL, IS THE PROPERTY  
 OF COASTAL ENGINEERING ASSOCIATES, INC.  
 AND IS NOT TO BE REPRODUCED OR  
 TRANSMITTED IN ANY FORM OR BY ANY  
 MEANS, ELECTRONIC OR MECHANICAL,  
 INCLUDING PHOTOCOPYING, RECORDING,  
 OR BY ANY INFORMATION STORAGE AND  
 RETRIEVAL SYSTEM, WITHOUT THE  
 WRITTEN PERMISSION OF COASTAL  
 ENGINEERING ASSOCIATES, INC.

DATE	REV	BY	CHKD	APP'D	REVISION
11/19/19	1	1	1	1	
11/19/19	2	1	1	1	
11/19/19	3	1	1	1	
11/19/19	4	1	1	1	
11/19/19	5	1	1	1	
11/19/19	6	1	1	1	
11/19/19	7	1	1	1	
11/19/19	8	1	1	1	
11/19/19	9	1	1	1	
11/19/19	10	1	1	1	

SHEET

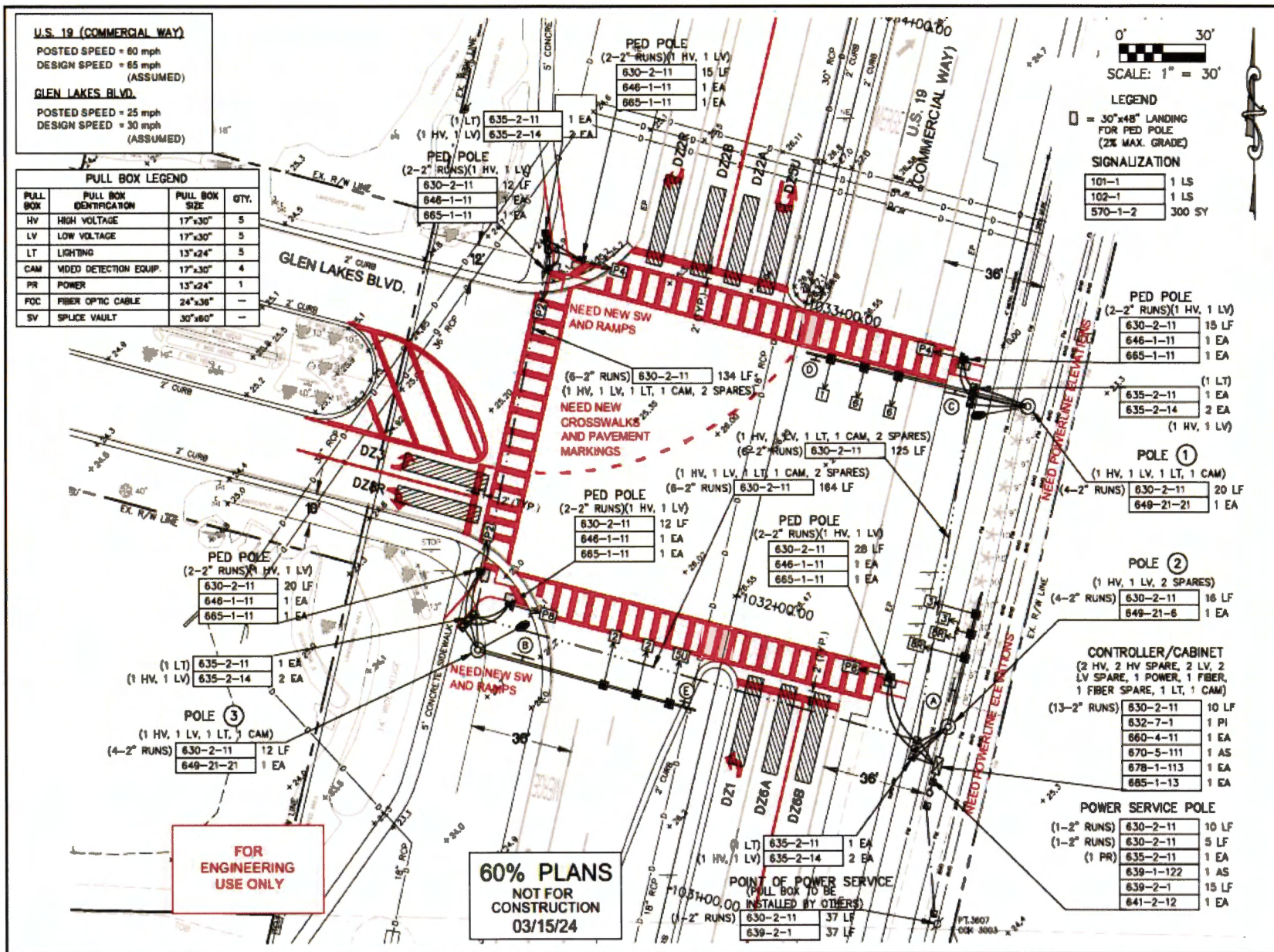
2

21093



POSTED SPEED = 25 mph  
DESIGN SPEED = 30 mph  
(ASSUMED)


PULL BOX LEGEND			
PULL BOX	PULL BOX IDENTIFICATION	PULL BOX SIZE	QTY.
HV	HIGH VOLTAGE	17"x30"	5
LV	LOW VOLTAGE	17"x30"	5
LT	LIGHTING	13"x24"	5
CAM	VIDEO DETECTION EQUIP.	17"x30"	4
PR	POWER	13"x24"	1
FOC	FIBER OPTIC CABLE	24"x36"	—
SV	SPLICE VAULT	30"x60"	—



**FOR  
ENGINEERING  
USE ONLY**

**60% PLANS**  
**NOT FOR**  
**CONSTRUCTION**  
**03/15/24**

0° 30'



SCALE: 1" = 30'

### LEGEND

□ = 30"x48" LANDING  
FOR PED POLE  
(2% MAX. GRADE)

## SIGNALIZATION

101-1	1 LS
102-1	1 LS
570-1-2	300 SY

PED POLE	
2-2* RUNS)(1 HV, 1 LV)	
630-2-11	15 LF
646-1-11	1 EA
665-1-11	1 EA

POLE ①

NS)	630-2-11	20 LF
	648-21-21	1 EA

POLE ②

RUNS)	630-2-11	16 LF
	649-21-6	1 EA

**CONTROLLER/CABINET**  
(2 HV, 2 HV SPARE, 2 LV, 2  
LV SPARE, 1 POWER, 1 FIBER  
1 FIBER SPARE, 1 LT, 1 CAM)

(13-2" RUNS)	830-2-11	10 LF
	832-7-1	1 PI
	860-4-11	1 EA
	670-5-111	1 AS
	678-1-113	1 EA
	685-1-13	1 EA

POWER SERVICE POLE

(1-2 <sup>nd</sup> RUNS)	630-2-11	10 LF
(1-2 <sup>nd</sup> RUNS)	630-2-11	5 LF
(1 PR)	635-2-11	1 EA
	639-1-122	1 AS
	639-2-1	15 LF
	641-2-12	1 EA

LINKS &amp; ASSOCIATES, INC.

[illegible]

**SIGNALIZATION PLANS**  
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

# SIGNALIZATION PLAN

[illegible]

**Composite Exhibit “C”**

**Plans and Specifications**

THIS ITEM HAS BEEN DIGITALLY SIGNED AND SEALED BY

ON THE DATE ADJACENT TO THE SEAL  
PRINTED COPIES OF THIS DOCUMENT ARE  
NOT CONSIDERED SIGNED AND SEALED  
AND THE SIGNATURE MUST BE VERIFIED  
ON ANY ELECTRONIC COPIES.



# SIGNALIZATION PLANS

## FOR

### U.S. 19 (Commercial Way) and Glen Lakes Blvd.

Hernando County, Florida

PREPARED FOR

Glen Lakes Common, LLC  
13904 N. Boulevard  
Tampa, Florida 33613



VICINITY MAP

SECTION 13, TOWNSHIP 22 SOUTH, RANGE 17 EAST  
HERNANDO COUNTY, FLORIDA  
FDOT HIGHWAY SEGMENT #08-020-000  
U.S. 19 MILEPOST (M.P. 10.635)  
ACCESS CLASSIFICATION 3  
POSTED SPEED: 60mph

#### GOVERNING STANDARD PLANS:

FLORIDA DEPARTMENT OF TRANSPORTATION, FY 2024-25 STANDARD PLANS FOR ROAD AND BRIDGE  
CONSTRUCTION AND APPLICABLE INTERIM REVISIONS (IRs).  
STANDARD PLANS FOR ROAD CONSTRUCTION AND ASSOCIATED IRs ARE AVAILABLE AT THE  
FOLLOWING WEBSITE:  
[http://www.flot.com/design/standard\\_plans](http://www.flot.com/design/standard_plans)

APPLICABLE IRs: NONE  
STANDARD PLANS FOR BRIDGE CONSTRUCTION ARE INCLUDED IN THE STRUCTURES PLANS  
COMPONENT.

#### GOVERNING STANDARDS SPECIFICATIONS:

FLORIDA DEPARTMENT OF TRANSPORTATION, FY 2024-25 STANDARD SPECIFICATIONS FOR ROAD AND  
BRIDGE CONSTRUCTION AT THE FOLLOWING WEBSITE:  
<http://www.flot.com/standards/specifications/implemented/SignBooks>

60% PLANS  
NOT FOR  
CONSTRUCTION  
12/20/24

FDOT CONSTRUCTION AGREEMENT #2024-C-798-XXXXX

LINCKS & ASSOCIATES, LLC  
ENGINEERS PLANNERS  
5023 WEST LAUREL STREET  
TAMPA, FLORIDA 33607  
PH. (813) 289-0039  
Lic. No. EB0004638



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T-13	Traffic Control General Notes
T-14	Subsurface Utility Engineering Plan

DATE: 03/13/24	SIGNED:	JOB NO: 22050
REVISION	Robert W. Fulp PE No. 44674	SHEET T-1 OF T-14



**LINKS & ASSOCIATES, LLC**  
5023 West Laurel Street  
Tampa, Florida 33607  
Tel (813) 288-0039  
Fax (813) 287-0674  
Lic. No. EB0004638



DATE	03/13/24
DESIGNED	OAR/RWF
DRAWN	OAR
CHECKED	RWF

**SIGNALIZATION PLANS**  
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

## TABULATION

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**T-2 of T-**

**60% PLANS  
NOT FOR  
CONSTRUCTION  
12/20/24**

**T-3 of T-**

ALL WORK PERFORMED IN THE DEPARTMENT'S RIGHT OF WAY SHALL BE DONE IN ACCORDANCE WITH THE MOST CURRENT EDITIONS OF:

- 1) ALL AREAS IN THE FDOT RIGHT OF WAY DISTURBED DURING CONSTRUCTION SHALL BE RESTORED, COMPACTED, SODDED AND WATERED IN CONFORMANCE WITH THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 570 AND 981 AND DESIGN STANDARDS INDEX 570-010.

3) ALL TRAFFIC STRIPES AND PAVEMENT MARKINGS ARE TO BE LEAD-FREE, NON-SOLVENT BASED THERMOPLASTIC. THE PERMITTEE SHALL FURNISH THE DEPARTMENT WITH THE MANUFACTURER'S CERTIFICATION THAT THE THERMOPLASTIC IS 'LEAD FREE'. REMOVAL OF EXISTING STRIPING SHALL BE DONE BY MILLING AND RESURFACING OF THE FRICTION/SURFACE COURSE TO OBLITERATE OBSOLETE PAVEMENT MARKINGS.

5) THE FDOT REQUIRES DOCUMENTATION FOR SUCCESSFUL COMPLETION OF AN APPROVED WORK ZONE TRAFFIC CONTROL TRAINING COURSE FOR THE AGENCY, UTILITY OR CONTRACTOR EMPLOYEE(S) INSTALLING AND/OR MAINTAINING THE APPROVED MAINTENANCE OF TRAFFIC PLAN. DOCUMENTATION IS TO BE FURNISHED TO THE DEPARTMENT AT THE PRE-CONSTRUCTION MEETING OR BEFORE OCCUPYING STATE RIGHT-OF-WAY.

7) SILT FENCE FOR EROSION CONTROL IN CONFORMANCE WITH THE FLORIDA EROSION SEDIMENT CONTROL MANUAL IS REQUIRED FOR ALL AREAS OF CONSTRUCTION WITHIN FDOT R/W, PERPENDICULAR TO AND PARALLEL WITH THE STATE ROAD.

A) INLINE TRUNCATED DOME EZ TILE SUPPLIED BY PROFESSIONAL PAVEMENT PRODUCTS  
B) TOPMARK SUPPLIED BY FLINT TRADING  
C) VANGUARD TRUNCATED DOME SUPPLIED BY VANGUARD

10) FDOT REQUIRES A PIPE VIDEO BE DONE ON ALL PIPES ASSOCIATED WITH A PERMIT PROJECT IN FDOT ROW. SPEC 430-4.8.

ALLEN TURNER  
352-540-8219  
HERNANDO COUNTY  
UTILITY DEPT.  
15385 CORTEZ BLVD.  
BROOKSVILLE, FL 34801

5023 West Laurel Street  
Tampa, Florida 33607  
Tel (813) 289-0039  
Fax (813) 287-0674  
Lic. No. EB0004638



**SIGNALIZATION PLANS**  
**U.S. 19 (Commercial Way) & Glen Lakes Blvd.**

## GENERAL NOTES

[illegible]

SHEET T-4 of T-

**PRIOR TO CONSTRUCTION  
48 HOURS BEFORE DIGGING  
CALL  
TOLL FREE  
1-800-432-4770  
SUNSHINE STATE ONE CALL CENTER**

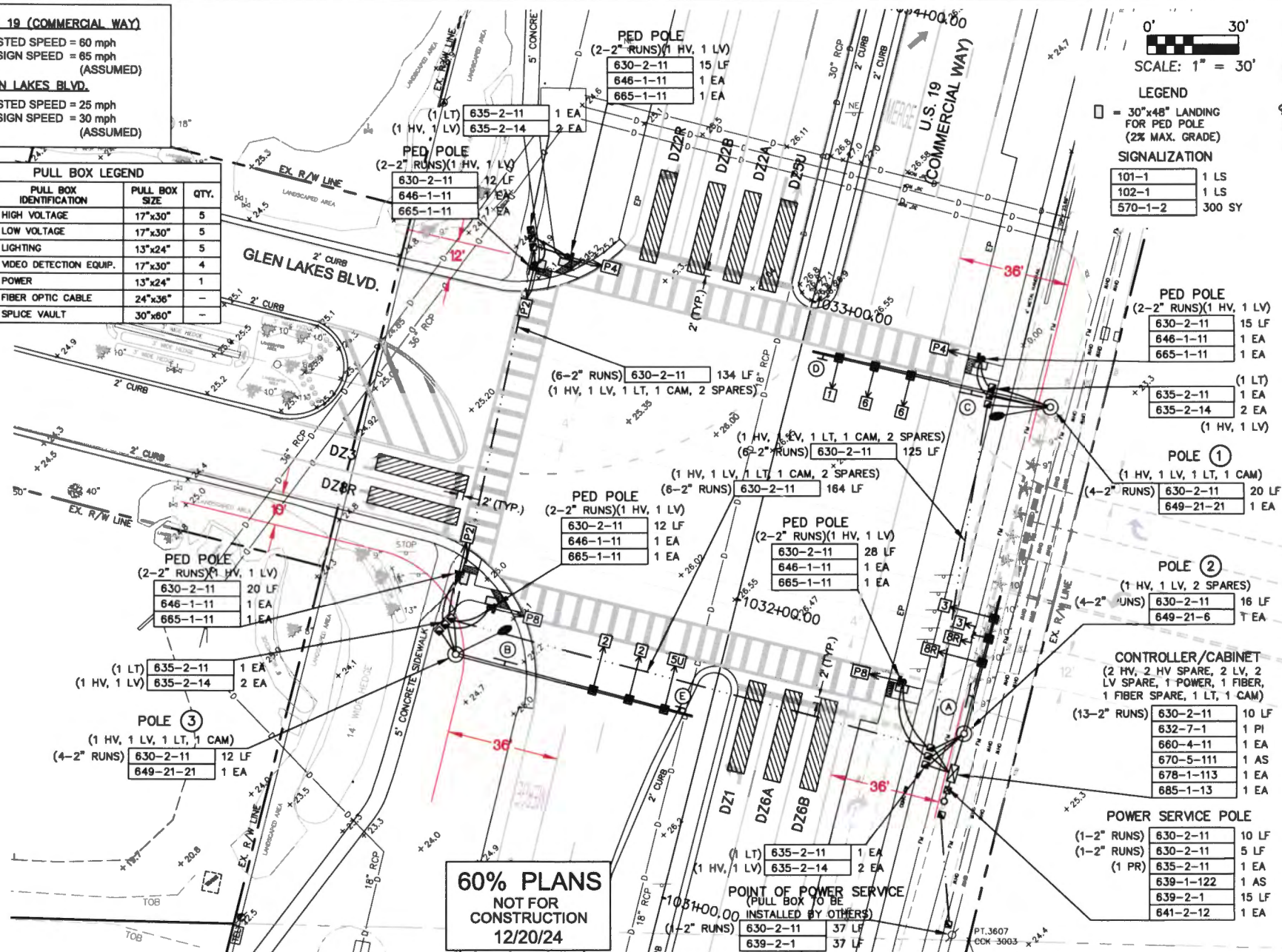
**60% PLANS**  
NOT FOR  
CONSTRUCTION  
12/20/24



POSTED SPEED = 60 mph  
DESIGN SPEED = 65 mph  
(ASSUMED)


POSTED SPEED = 25 mph  
DESIGN SPEED = 30 mph  
(ASSUMED)

PULL BOX	PULL BOX IDENTIFICATION	PULL BOX SIZE	QTY.
HV	HIGH VOLTAGE	17"x30"	5
LV	LOW VOLTAGE	17"x30"	5
LT	LIGHTING	13"x24"	5
CAM	VIDEO DETECTION EQUIP.	17"x30"	4
PR	POWER	13"x24"	1
FOC	FIBER OPTIC CABLE	24"x36"	-
SV	SPLICE VAULT	30"x60"	-



**60% PLANS**  
**NOT FOR**  
**CONSTRUCTION**  
**12/20/24**

0' 30'



SCALE: 1" = 30'

LEGEND  
30"x48" LANDING  
FOR PED POLE  
(2% MAX. GRADE)

SIGNALIZATION	
101-1	1 LS
102-1	1 LS
570-1-2	300 SY

PED POLE	
-2" RUNS	(1 HV, 1 LV)
630-2-11	15 LF
646-1-11	1 EA
665-1-11	1 EA
	(1 LT)
635-2-11	1 EA
635-2-14	2 EA
	(1 HV, 1 LV)

POLE ①		
V, 1 LV, 1 LT, 1 CAM)		
S)	630-2-11	20 L
	649-21-21	1 EA

POLE (2)  
(V, 1 LV, 2 SPARES)

630-2-11	16 LF
649-21-6	TEA

CONTROLLER/CABINET		
(2 HV, 2 HV SPARE, 2 LV, 2 LV SPARE, 1 POWER, 1 FIBER, 1 FIBER SPARE, 1 LT, 1 CAM)		
(13-2" RUNS)	630-2-11	10 LF
	632-7-1	1 PI
	680-4-11	1 EA
	670-5-111	1 AS
	678-1-113	1 EA
	685-1-13	1 EA

POWER SERVICE POLE		
(1-2" RUNS)	630-2-11	10 LF
(1-2" RUNS)	630-2-11	5 LF
(1 PR)	635-2-11	1 EA
	639-1-122	1 AS
	639-2-1	15 LF
	641-2-12	1 EA

LINCKS &amp; ASSOCIATES, LLC



DATE	08/18/24
DESIGNED	QAR/RWF
DRAWN	QAR
CHECKED	RWF
JOB NO.	00000

**SIGNALIZATION PLANS**  
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

# SIGNALIZATION PLAN

[illegible]

SHEET T-5 OF T-14

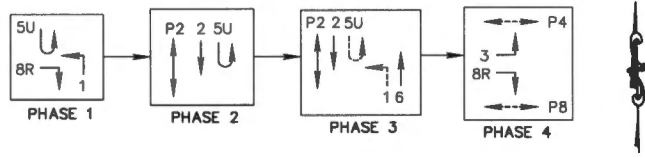
CONTROLLER NOTES

- THE MAJOR STREET IS US 19 (COMMERCIAL WAY), PHASE 3 (MOVEMENTS 2 AND 6). THE MINOR STREET IS GLEN LAKES BLVD., PHASE 4 (MOVEMENT 3 AND 8).
- THE CONTROLLER CABINET SHALL BE CAPABLE OF RUNNING A SOP 10. THE INITIAL SOP SHALL BE SOP 7 (MODIFIED), AS SHOWN, WITH THE FOLLOWING:
  - (A) FLASHING OPERATION IS YELLOW FOR MOVEMENTS 2 AND 6 AND RED FOR ALL OTHER MOVEMENTS.
  - (B) COORDINATION ON PHASE 3 (MOVEMENTS 2 AND 6).
  - (C) REFER TO GENERAL NOTES SHEET FOR ADDITIONAL CONTROLLER REQUIREMENTS.
  - (D) SIGNAL TIMING MAY REQUIRE FIELD ADJUSTMENTS BY THE COUNTY ENGINEER.

GENERAL NOTES

- THE POLE LOCATIONS (STATION / OFFSET) ARE MEASURED FROM THE BASELINE OF SURVEY ALONG U.S. 19 (COMMERCIAL WAY).
- ALL VEHICULAR SIGNAL HEADS SHALL HAVE BACKPLATES WITH REFLECTIVE BORDERS AND TUNNEL VISORS.
- POWER SERVICE METER BASE AND DISCONNECT SHALL BE INSTALLED ON THE CONCRETE SERVICE POLE AS SHOWN ON THE PLANS AND PER INDEX NUMBERS 17504 AND 17738.
- THE CCTV CAMERA SHALL BE MOUNTED TO THE TOP OF POLE #2 USING A 10 FT. LONG SIGN BRACKET ARM. ANY NECESSARY CABLE SUPPORTS REQUIRED WITHIN THE SIGNAL POLE SHALL BE INCIDENTAL TO THE COST OF THE SIGNAL POLE. THE CCTV CAMERA CABLES SHALL RUN TO THE CONTROLLER CABINET THROUGH A SEPARATE PULL BOX AND CONDUIT AS SHOWN ON PLANS. THE CABINET SHALL BE WIRED TO ACCOMMODATE THE CCTV CAMERA.
- ONE (1) DISCONNECT WITH TWO (2) BREAKERS (ONE LABELED SIGNALS AND ONE LABELED LIGHTING) SHALL BE INSTALLED ON THE POWER SERVICE POLE AS INDICATED ON THE PLANS.

S.O.P. 7 MODIFIED



DETECTION DEVICES

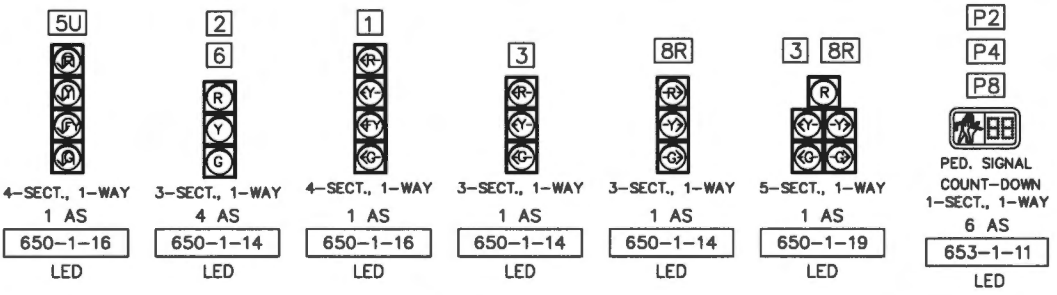
660-4-12 2 EA

VEHICULAR SIGNAL HEAD DETAILS

PEDESTRIAN SIGNAL HEAD DETAILS

POLE LOCATIONS

POLE NO.	①	②	③
STATION	1032+85.70	1031+75.70	1031+60.55
OFFSET	83.4' RT	82.0' RT	84.0' LT
ELEV. AT GROUND	22.30	24.45	24.45
ELEV. AT TOP FOUNDATION	22.80	24.95	24.95
HIGHEST ELEVATION OF ROADWAY BENEATH SPAN WIRE	26.55	22.60	26.25



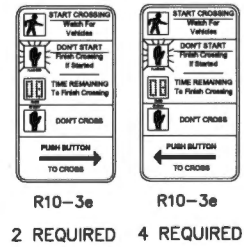
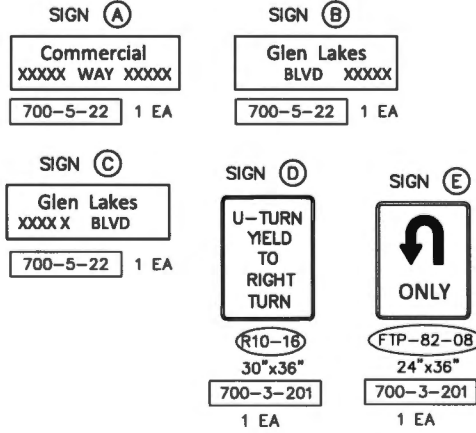
VIDEO VEHICLE DETECTION ASSIGNMENTS

CAMERA DETECTOR	DETECTION ZONE	DETECTION SIZE	CONNECT TO MOVEMENT	DELAY TIME (SEC.)
CD1	DZ2R	6'x30'	2	-
CD1	DZ2A	6'x30'	2	-
CD1	DZ2B	6'x30'	2	-
CD1	DZ5U	6'x30'	5	-
CD2	DZ3	6'x30'	3	-
CD2	DZ8R	6'x30'	8	-
CD2	DZ6A	6'x30'	6	-
CD2	DZ6B	6'x30'	6	-
CD2	DZ1	6'x30'	6	-

CONTROLLER TIMINGS

MOVEMENT NUMBER	1	2	3	4	5	6	7	8
MINIMUM GREEN	7	20	7	-	7	20	-	7
EXTENSION	3	3	3	-	3	3	-	3
MAXIMUM GREEN 1	TO BE DETERMINED BY HERNANDO COUNTY							
MAXIMUM GREEN 2								
YELLOW CLEARANCE	5.5	5.5	5.5	-	5.5	5.5	-	5.5
ALL RED	2.0	2.7	2.5	-	2.0	2.0	-	2.5
PEDESTRIAN WALK	-	7	-	7	-	-	-	7
PED. CLEARANCE	-	38	-	38	-	-	-	38
RECALL	-	MIN	-	-	-	MIN	-	-

SIGN DETAILS



60% PLANS NOT FOR CONSTRUCTION 12/20/24

LINKS & ASSOCIATES, LLC  
5023 West Laurel Street  
Tampa, FL 33607  
Tel: (813) 281-0039  
Fax: (813) 287-0874  
Lic. No. EB0004638

ENGINEERS  
PLANNERS

DATE: 08/15/24  
DESIGNED: CAR/RWF  
DRAWN: CAR  
CHECKED: RWF

SIGNALIZATION PLANS  
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

SIGNALIZATION

REVISIONS

SHEET T-6 OF T-



# U.S. 19 (COMMERCIAL WAY)

POSTED SPEED = 60 mph  
DESIGN SPEED = 85 mph  
(ASSUMED)

# GLEN LAKES BLVD.

POSTED SPEED = 25 mph  
DESIGN SPEED = 30 mph  
(ASSUMED)

## LIGHT POLE (A)

STAT. 17+98.85  
OFFSET 168.8' LT

630-2-11	7 LF
715-1-12	21 LF
715-4-13	1 EA
715-500-1	1 EA

## LIGHT POLE (B)

STAT. 1033+26.65  
OFFSET 79.3' LT

630-2-11	30 LF
635-2-11	1 EA
715-1-12	90 LF
715-4-13	1 EA
715-500-1	1 EA

## INTERSECTION LIGHTING NOTES:

1. FOR LUMINAIRES ATTACHED TO THE SIGNAL POLES, PLEASE SEE SHEET T-10.
2. FOR LIGHT POLES A, B, C AND D THE LUMINAIRES SHALL BE IES CURVE ATB2\_P801\_R2\_3K OR EQUAL AND BE MOUNTED ON A 15 FT. BRACKET ARM WITH A MOUNTING HEIGHT OF 40 FT.
3. THE LIGHTING CONDUCTORS SHALL RUN IN A SEPARATE CONDUIT. A DEDICATED STREET LIGHTING PULL BOX SHALL BE LOCATED IN EACH QUADRANT OF THE INTERSECTION.
4. LIGHT POLES A AND D SHALL BE INSTALLED AT LEAST 14 FT. FROM THE EDGE OF THE TRAVEL LANE. LIGHT POLES B AND C SHALL BE INSTALLED AT LEAST 20' FROM THE EDGE OF TRAVEL LANE. THE ENGINEER-OF-RECORD SHALL BE NOTIFIED IF THE LOCATION OF THE LIGHT POLE HAS CHANGED.
5. PLEASE BE ADVISED THAT NO SUBSURFACE UTILITY EXPLORATION WAS CONDUCTED FOR THE PROPOSED LOCATION OF THE LIGHT POLES A, B, C AND D.
6. LIGHT POLE A SHALL HAVE BREAKAWAY / FRANGIBLE BASES.

## LIGHT POLE (D)

STAT. 1031+82.30  
OFFSET 111.8' LT

630-2-11	25 LF
635-2-11	1 EA
715-1-12	75 LF
715-4-13	1 EA
715-500-1	1 EA

## POLE (3)

715-1-12	36 LF
715-5-32	1 EA
715-500-1	1 EA

## POLE (1)

715-1-12	60 LF
715-5-32	1 EA
715-500-1	1 EA

## LIGHT POLE (C)

STAT. 1031+47.75  
OFFSET 59.6' RT

630-2-11	20 LF
635-2-11	1 EA
715-1-12	60 LF
715-4-13	1 EA
715-500-1	1 EA

60% PLANS

NOT FOR  
CONSTRUCTION  
12/20/24

LINKS & ASSOCIATES, LLC

5023 West Laurel Street  
Tampa, Florida 33607  
Tel (813) 289-0039  
Fax (813) 287-0674  
Lic. No. EB0004636



DATE	08/19/24
DRAWN	CAR/RWF
CHECKED	RWF
JOB NO.	250080

SIGNALIZATION PLANS  
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

## LIGHTING PLAN

SHEET	T-7 of T-14
-------	-------------

Technical drawing of a rectangular sign. The sign features the text "Commercial" in a large, bold, sans-serif font, with "XXXXX WAY XXXXX" in a smaller, bold, sans-serif font below it. The drawing includes various dimension lines and callouts:

- Overall width: 8'-0"
- Overall height: 2'-0"
- Top-left corner radius: 14.8"
- Left side dimensions (from top): 8"E, 3.2"
- Right side dimensions (from top): 3.2", 8"EM, 3.55", 8"E, 3.25"
- Bottom-left corner radius: 4"
- Bottom-right corner radius: 4"
- Bottom width dimension: 88"

[illegible]

Technical drawing of a street sign for Glen Lakes XXXXX BLVD. The sign is rectangular with a black border. The text "Glen Lakes" is on the top line and "XXXXX BLVD" is on the bottom line, both in bold, black, sans-serif font. Dimensions are provided: overall width is 8'-0", overall height is 2'-0". The sign face is 78.6" wide and 3.6" high. The text "Glen Lakes" is 3.2" high, and "XXXXX BLVD" is 3.6" high. The sign is mounted on a post with a 3.2" diameter. The sign is 3.2" from the top and bottom edges of the post. The sign is 3.2" from the left and right edges of the post. The sign is 3.2" from the top and bottom edges of the post.

[illegible]

Technical drawing of a rectangular sign for "Glen Lakes BLVD XXXXX". The drawing shows the sign's dimensions and mounting hardware. The sign is 8'-0" wide and 3'-2" high. The text "Glen Lakes" is in a large, bold, sans-serif font, and "BLVD XXXXX" is in a smaller, bold, sans-serif font below it. The sign is mounted on a wall with four screws, two on each side. The mounting hardware dimensions are: 2'-0" for the top and bottom screws, 18.7" for the left side screws, and 78.3" for the right side screws. The sign is 3.2" from the top and bottom edges, 6" from the left and right edges, and 3.68" from the top and bottom edges. The sign is 6" from the left and right edges.

[illegible]

<p>(NOT USED)</p>									
NO. OF LEAF PICTURES	PICTURE SPACING	PHOTOMETRIC CURVE				BATT	VOLTAGE		

[illegible]

**60% PLANS  
NOT FOR  
CONSTRUCTION  
12/20/24**









TO BE COMPLETED  
FOR FINAL SUBMITTAL

60% PLANS  
NOT FOR  
CONSTRUCTION  
12/20/24

REVISIONS	

SIGNALIZATION PLANS  
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

DATE	03/13/24
DESIGNED	
DRAWN	
CHECKED	

BY OTHERS

REPORT OF  
CARTER MORRISON

GENERAL NOTES

- 1. ALL TRAFFIC CONTROL SHALL CONFORM TO FDOT STANDARD PLANS, INDEX 102-600 SERIES, AND THE FEDERAL HIGHWAY ADMINISTRATION'S MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CURRENT EDITION).
- 2. PRIOR TO COMMENCEMENT OF CONSTRUCTION OPERATIONS, INSTALL ADVANCE WARNING SIGNS, BARRICADES AND CONSTRUCTION SIGNS UTILIZING FDOT STANDARD PLANS, INDEX 600 SERIES.
- 3. UTILIZE STANDARD PLANS 102-600, 102-613, 102-615, and 102-660, AND OTHER INDEXES AS APPLICABLE FOR MAINTAINING TRAFFIC DURING CONSTRUCTION ACTIVITIES.
- 4. FOR WORK WITHIN FDOT RIGHT-OF-WAY, THE FDOT REQUIRES DOCUMENTATION FOR SUCCESSFUL COMPLETION OF AN APPROVED WORK ZONE TRAFFIC CONTROL TRAINING COURSE FOR THE AGENCY, UTILITY, OR CONTRACTOR EMPLOYEE(S) DESIGNING, INSTALLING, AND/OR MAINTAINING THE APPROVED MAINTENANCE OF TRAFFIC PLAN IN ACCORDANCE WITH DEPARTMENT PROCEDURE, TOPIC NO. 625-010-010.
- 5. FOR WORK WITHIN FDOT RIGHT-OF-WAY, THE CONTRACTOR SHALL PROVIDE A TEMPORARY TRAFFIC CONTROL PLAN PREPARED UNDER THE DIRECTION OF, AND SIGNED AND SEALED BY, A LICENSED FLORIDA PROFESSIONAL ENGINEER WHO IS EXPERIENCED IN PREPARING TRAFFIC CONTROL PLANS AND WHO IS CERTIFIED PER FDOT PROCEDURE, TOPIC NO. 625-010-010.
- 6. THE CONTRACTOR'S TRAFFIC CONTROL PLAN MUST RECEIVE THE APPROVAL OF THE FLORIDA DEPARTMENT OF TRANSPORTATION PRIOR TO IMPLEMENTATION.
- 7. ALL TEMPORARY TRAFFIC CONTROL DEVICES FOR THE FOLLOWING FACILITIES SHALL BE DESIGNED AND INSTALLED TO MEET THE EXISTING POSTED SPEEDS AS STATED FOR ALL TRAFFIC CONTROL PHASES.
- 8. THE EXISTING POSTED SPEED LIMIT OF 60 MPH ALONG U.S. 19 (COMMERCIAL WAY) AND 25 MPH ALONG GLEN LAKES BOULEVARD SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION.
- 9. ONLY SINGLE LANE CLOSURES WILL BE PERMITTED. LANE CLOSURES WILL NOT BE PERMITTED FROM THE HOURS OF 7:00 AM TO 9:00 PM AND FROM 4:00 PM TO 6:00 PM.
- 10. LANE CLOSURE RESTRICTIONS SHALL APPLY EQUALLY TO BOTH MOTOR VEHICLE AND BICYCLE TRAFFIC.
- 11. THE CONTRACTOR SHALL UTILIZE PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) DURING ACTIVE NIGHT-TIME OPERATIONS. THE DISPLAY SHALL BE PLACED IN ADVANCE OF THE FIRST WORK ZONE SIGN IN EACH DIRECTION APPROACHING THE WORK ZONE IN A LOCATION APPROVED BY THE ENGINEER. THE FOLLOWING PCMS DISPLAYS SHALL BE UTILIZED (SEE BELOW):



SIGNS TO BE LOCATED 800' IN ADVANCE OF CONSTRUCTION ZONE AND PLACED SEVEN DAYS PRIOR TO START OF CONSTRUCTION.



SIGNS TO BE LOCATED 500' IN ADVANCE OF CONSTRUCTION ZONE AND SHALL REMAIN FOR THE DURATION OF CONSTRUCTION.

60% PLANS  
NOT FOR  
CONSTRUCTION  
12/20/24

LINKS & ASSOCIATES, LLC

5023 West Laurel Street  
Tampa, Florida 33607  
Tel (813) 288-0039  
Fax (813) 288-0074  
U.E. No. EB0004036

DATE	08/15/24
DESIGNED	OAR/RWF
DRAWN	OAR
CHECKED	RWF

SIGNALIZATION PLANS

U.S. 19 (Commercial Way) & Glen Lakes Blvd.

TRAFFIC CONTROL

REVISIONS


SHEET

T-13 of T-

**TO BE COMPLETED  
FOR FINAL SUBMITTAL**

**60% PLANS**  
NOT FOR  
CONSTRUCTION  
**12/20/24**

[illegible]

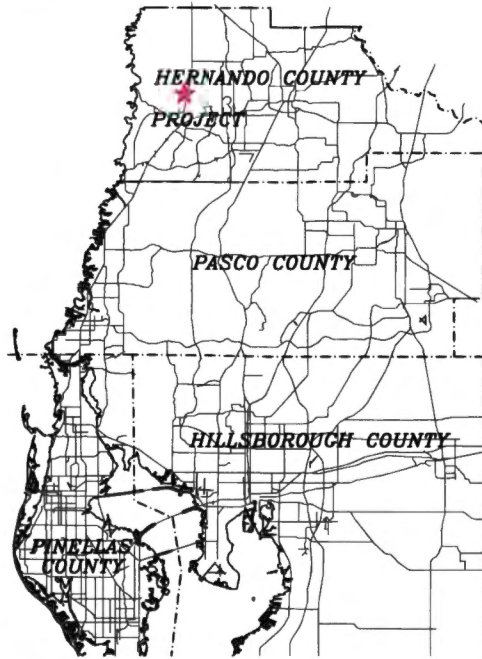
**SIGNALIZATION PLANS**  
**U.S. 19 (Commercial Way) & Glen Lakes Blvd.**

## SUBSURFACE UTILITY

DATE	03/13/24
DESIGNED	OAR/RWFF
DRAWN	OAR
CHECKED	RWFF

## BY OTHERS

FDOT ACCESS PERMIT NO. \_\_\_\_\_  
 FDOT DRAINAGE PERMIT NO. \_\_\_\_\_  
 SWFWMD EXEMPTION NO. \_\_\_\_\_



# ROADWAY IMPROVEMENTS

## FOR

# U.S. 19 (Commercial Way) and Glen Lakes Blvd

HERNANDO COUNTY, FLORIDA

PREPARED FOR

## Glen Lakes Common, LLC

13904 North Boulevard  
Tampa, Florida 33613



**VICINITY MAP**  
 SEC 13 T19N S22 S, R17 E  
 HERNANDO COUNTY, FLORIDA  
 SR55 SEGMENT 08020000  
 MP: 10.635

NEW IMPERVIOUS: 600 SF  
 CLEAR ZONE: 36' TRAVEL LANE,  
 24' AUXILIARY LANE

**LINCKS & ASSOCIATES, LLC.**  
**ENGINEERS PLANNERS**  
 5023 WEST LAUREL STREET  
 TAMPA, FLORIDA 33607  
 PH. (813) 289-0039  
 Lic. No. EB00004638



### INDEX

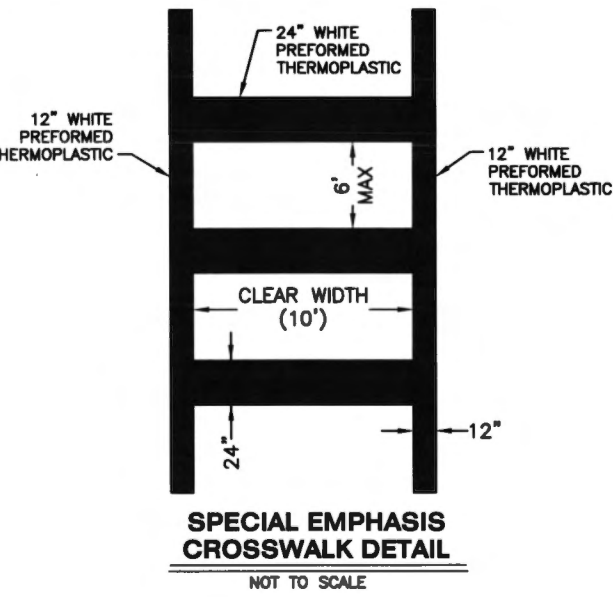
1	Cover Sheet
2	Key Sheet & Typical Sections
3	General Notes
4	Legends & Miscellaneous Details
5-6	Roadway Plans
7-8	Signing and Pavement Marking Plans
9	Guide Sign Worksheet

DATE: 12/29/24	SIGNED:	JOB NO: 22050
REVISION		SHEET 1
	David W. DeWitt PE No. 24760	OF 9









**UTILITY LEGEND**

EXISTING	PROPOSED	
--- W ---		WATERMAIN
--- NPW ---		RECLAIMED WATERMAIN
--- BFO ---		COMMUNICATIONS
--- S ---		FORCEMAIN
--- G ---		GAS
--- BT ---		BURIED TELEPHONE
--- BE ---		BURIED ELECTRIC
SEE PLANS, PROFILES & CROSS SECTIONS FOR SIZES		

CALL 48 HOURS BEFORE YOU DIG



**It's the Law!**  
**811 or 1-800-432-4770**

SUNSHINE STATE ONE CALL OF FLORIDA, INC.

EOP	-----	Edge of Pavement
ROW	-----	Right of Way
TOB	-----	Top of Bank
TOS	-----	Toe of Slope
Conc.	-----	Concrete
Elev.	-----	Elevation
I.E.	-----	Invert Elevation
RCP	-----	Reinforced Concrete Pipe
CIP	-----	Cast Iron Pipe
PVC	-----	Poly Vinyl Chloride
CMP	-----	Corrugated Metal Pipe
DIP	-----	Ductile Iron Pipe
SHWL	-----	Seasonal High Water Level
MEG	-----	Match Existing Grade
D.E.	-----	Drainage Easement
NC	-----	Normal Crown
FFE	-----	Finished Floor Elevation
SSMH	-----	Sanitary Sewer Manhole
C	-----	Centerline of Right-of-Way
P	-----	Property Line
co	-----	Clean Out

**ROADWAY LEGEND**

EXISTING	PROPOSED	
---□---	---■---	STORM DRAINAGE STRUCTURE
+ 15.00	15.00	ELEVATION
---15---	---15---	CONTOUR
~>	~>	DIRECTION OF SURFACE FLOW
	· · · · ·	STAKED EROSION CONTROL
		RIPRAP 3-4" DIA. 6'x6'x6" DEEP ON FABRIC (FDOT SPEC 530)

LINCKS & ASSOCIATES, LLC.

5023 West Laurel Street  
 Tampa, Florida 33607  
 Tel (813) 286-0039  
 Fax (813) 287-1874

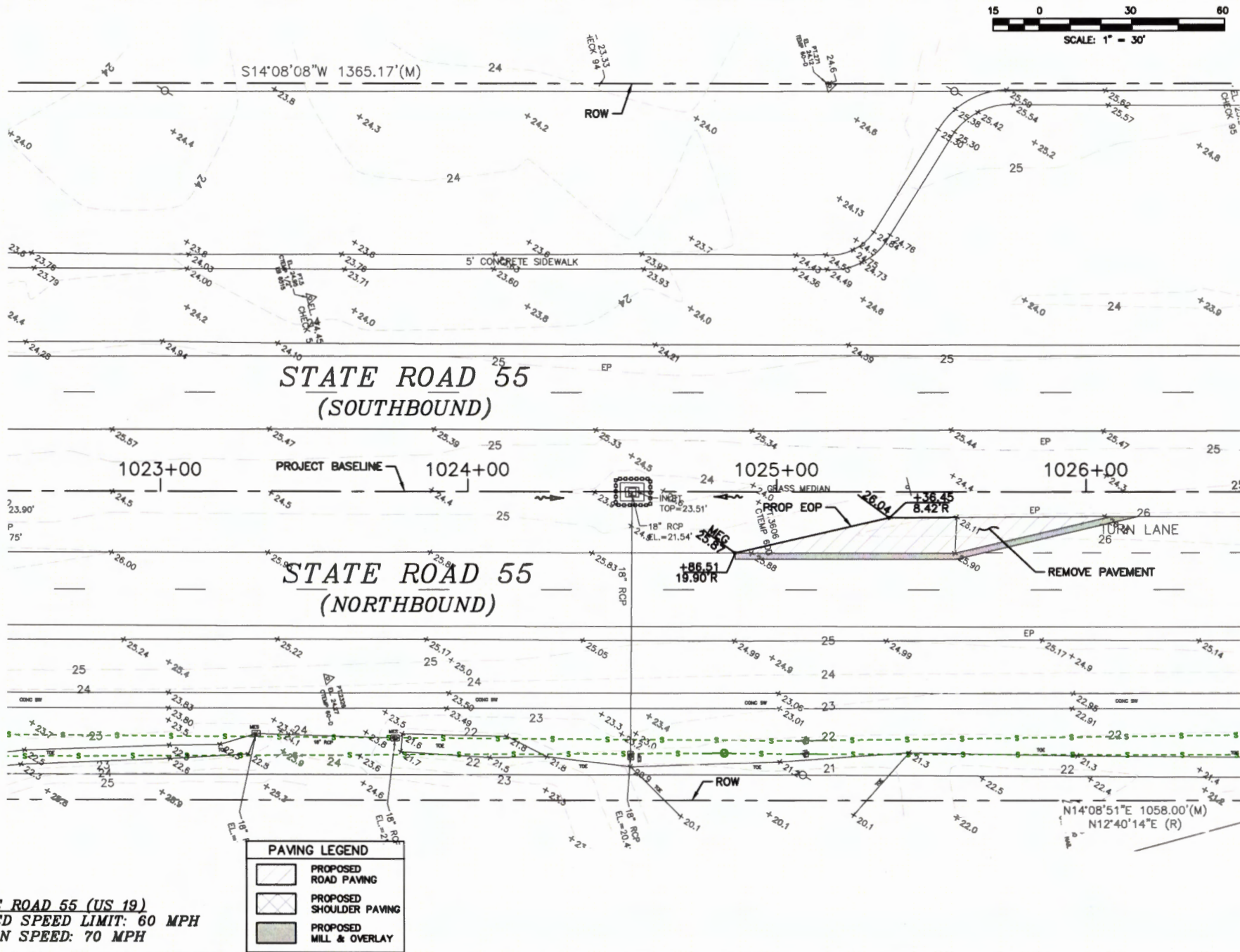


DATE	DESIGNED	DRAWN	CHECKED
12/20/24	JPA	JPA	DWD

**SR 55 Roadway Improvements**  
**Glen Lakes Blvd**  
**LEGENDS AND**

REVISIONS





LINKS & ASSOCIATES, LLC.  
 5023 West Laurel Street  
 Tampa, Florida 33607  
 Tel (813) 288-0039  
 Fax (813) 288-1074



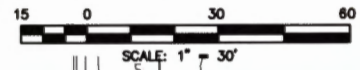
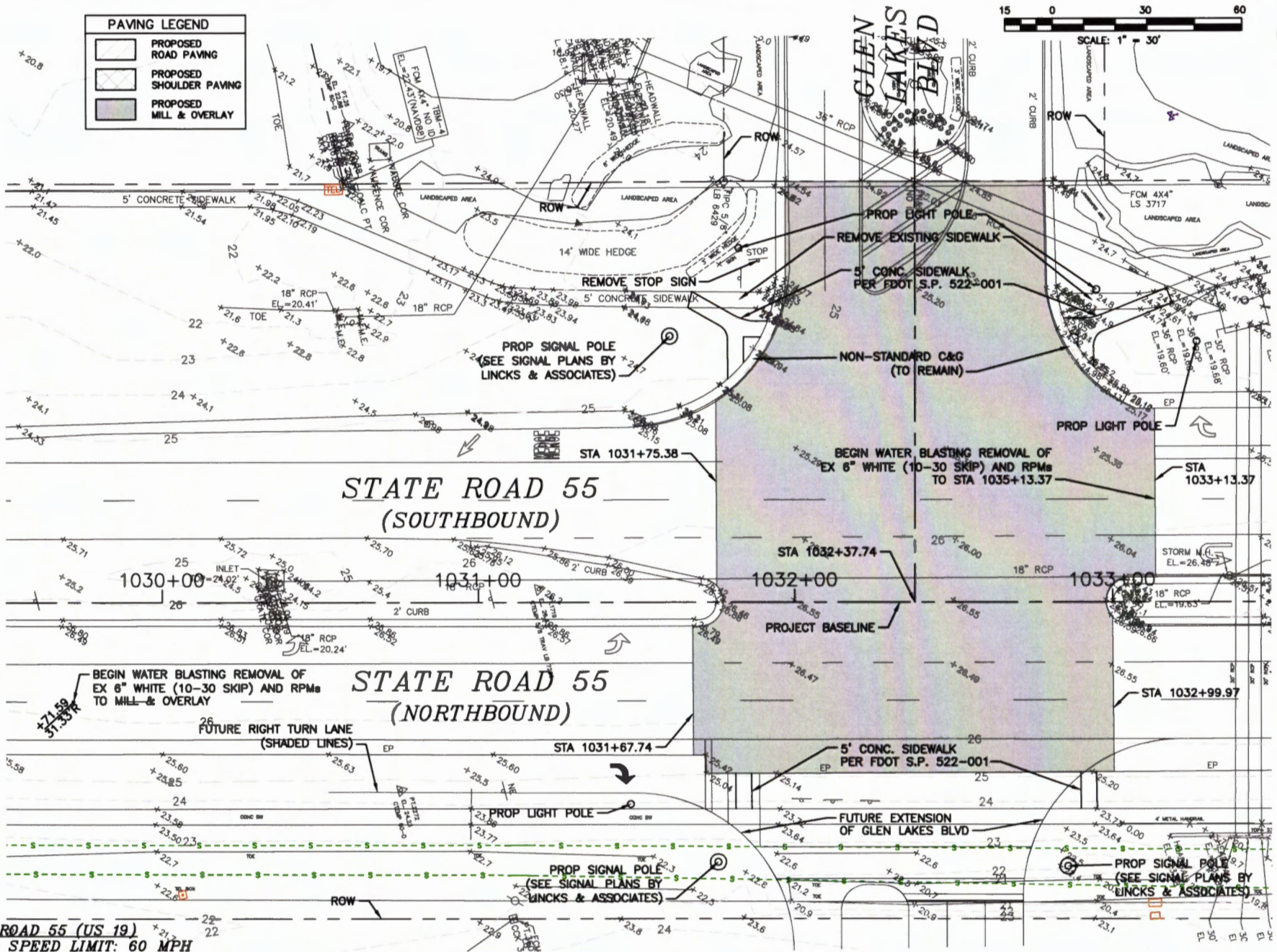
DATE	12/20/24
DESIGNED	JPA
DRAWN	JPA
CHECKED	DWD

**SR 55 Roadway Improvements**  
**Glen Lakes Blvd**  
**ROADWAY PLAN**

REVISIONS	SHEET
	5



PAVING LEGEND	
	PROPOSED ROAD PAVING
	PROPOSED SHOULDER PAVING
	PROPOSED MILL & OVERLAY



LINKS & ASSOCIATES, LLC.

5023 West Laurel Street  
Tampa, Florida 33607  
Tel (813) 289-0039  
Fax (813) 987-0674

Engineers

DATE	12/20/24	DESIGNED	JPA	DRAWN	JPA	CHECKED	DWD
------	----------	----------	-----	-------	-----	---------	-----

SR 55 Roadway Improvements  
Glen Lakes Blvd

ROADWAY PLAN

REVISIONS	

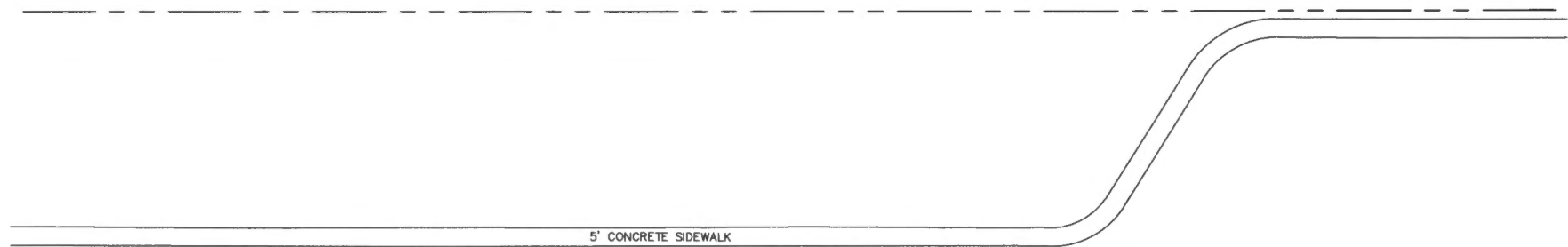
STATE ROAD 55 (US 19)

POSTED SPEED LIMIT: 60 MPH

DESIGN SPEED: 70 MPH

SHEET

6



*STATE ROAD 55*  
*(SOUTHBOUND)*

1023+00

1024+00

1025+00  
GRASS MEDIAN

1026+00

*STATE ROAD 55*  
*(NORTHBOUND)*

685' LEFT TURN LANE  
6" WHITE  
(6-10 SKIP)

+57.58  
19.88' R

MATCH EXISTING

LINCKS & ASSOCIATES, LLC.  
9023 West Loral Street  
Tampa, Florida 33607  
Tel (813) 288-0038  
Fax (813) 287-1874



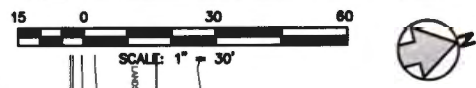
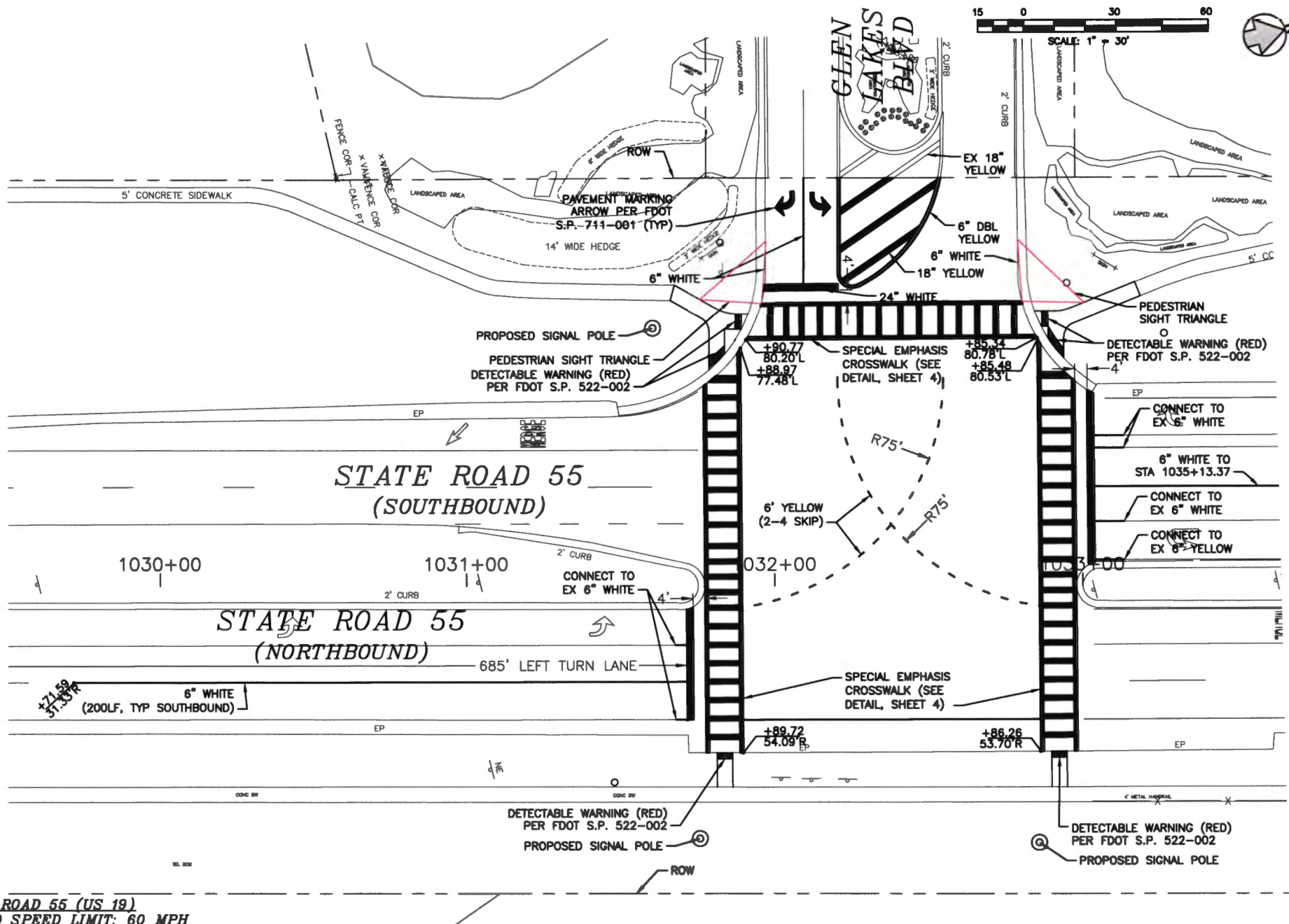
DATE	12/20/24
DESIGNED	JPA
DRAWN	JPA
CHECKED	DWD

SR 55 Roadway Improvements  
Glen Lakes Blvd  
SIGNING AND PAVEMENT

REVISIONS	SHEET
	7

STATE ROAD 55 (US 19)  
POSTED SPEED LIMIT: 60 MPH  
DESIGN SPEED: 70 MPH





REVISIONS

NO.	DATE	BY	CHKD	DESC

**SR 55 Roadway Improvements**  
**Glen Lakes Blvd**  
**SIGNING AND PAVEMENT**

DATE: 12/20/24  
DESIGNED: JPA  
DRAWN: JPA  
CHECKED: DWD

**LINKS & ASSOCIATES, LLC.**  
5023 West Laurel Street  
Tampa, Florida 33607  
Tel: (813) 288-0039  
Fax: (813) 287-1074

Engineers

SHEET **8**

**STATE ROAD 55 (US 19)**  
**POSTED SPEED LIMIT: 60 MPH**  
**DESIGN SPEED: 70 MPH**

**← Glen Lakes Blvd**

**NEXT SIGNAL**

NO. OF LIGHT FIXTURES	FIXTURE SPACING	POTENTIOMETRIC CURVE	MAT'T	VOLTAG

[illegible]

Technical drawing of a rectangular traffic sign for Glen Lakes Blvd. The sign is white with a black border and contains the text "Glen Lakes" in large bold letters, a right-pointing arrow, "Bld" in large bold letters, and "NEXT SIGNAL" in smaller bold letters. Dimensions are provided in feet and inches for the sign itself and its mounting hardware.

**Sign Dimensions:**

- Overall Width: 12'-0"
- Overall Height: 4'-0"
- Top Mounting Flange: 10.7" MIN
- Bottom Mounting Flange: 10.7" MIN
- Left Mounting Flange: 8" MIN
- Right Mounting Flange: 8" MIN
- Sign Face Width: 118.6"
- Sign Face Height: 36.65"

**Text on Sign:**

Glen Lakes →  
Bld  
NEXT SIGNAL



1. INSTALL THE ADVANCE STREET NAME SIGN ALONG U.S. 19 (COMMERCIAL WAY) FOR THE NORTHBOUND MOTORISTS AT LEAST 1,200 FT. SOUTH OF THE BEGINNING OF THE TAPER OF THE NORTHBOUND LEFT TURN LANE.
2. INSTALL THE ADVANCE STREET NAME SIGN ALONG U.S. 19 (COMMERCIAL WAY) FOR THE SOUTHBOUND MOTORISTS AT LEAST 1,200 FT. EAST OF THE BEGINNING OF THE TAPER OF THE SOUTHBOUND LEFT TURN LANE.
3. THE CONTRACTOR SHALL ENSURE THAT THE ADVANCE STREET NAME SIGNS ARE INSTALLED IN FRONT OF ANY OBSTRUCTION NEAR THE PROPOSED LOCATION THAT MAY OBSCURE THE VIEW OF THE SIGN.
4. REFER TO FDOT STANDARD PLAN INDEXES 700-020, FOR INSTALLATION REQUIREMENTS FOR MULTI-POST GROUND MOUNTED SIGNS
5. REFER TO FDOT STANDARD PLAN INDEX NO. 700-101 FOR PLACEMENT OF GROUND MOUNTED SIGNS
6. THE CONTRACTOR SHALL VERIFY THAT NO OBSTACLES ARE WITHIN THE PROPOSED SIGN COMPETING FOR THE PROPOSED ADVANCE STREET NAME SIGNS. AT LOCATIONS WHERE UNDERGROUND UTILITIES ARE IN CLOSE PROXIMITY TO SIGN FOUNDATIONS AS DETERMINED BY THE CONTRACTOR, THE CONTRACTOR SHALL HAND DIG THE FIRST FOUR FEET OF THE HOLE FOR THE SIGN POST FOUNDATION.
7. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE RESPONSIBILITY OF THE CONTRACTOR TO VERIFY THE LENGTH OF COLUMN SUPPORTS IN THE FIELD PRIOR TO FABRICATION.

LINCKS &amp; ASSOCIATES, LLC

5023 West Laurel Street  
Tampa, Florida 33607  
Tel (813) 269-0039  
Fax (813) 267-0674  
Lic. No. EB0004638



Roadway Improvements U.S. 19 (Commercial Way) & Glen Lakes Blvd.		DATE 08/26/24 DESIGNED CAG/RNMF DRAWN CAG CHECKED RNMF JOB NO. 080900
GUIDE SIGN WORKSHEET (ADVANCE STREET NAME SIGNS)		

**ENCLOSURE**

SHEET

**Exhibit “D”**

**Estimated Project Costs**



# ENGINEER'S CONSTRUCTION COST ESTIMATE

## SIGNALIZATION PROJECT

*U.S. 19 @ GLEN LAKES BOULEVARD*

*HERNANDO COUNTY, FLORIDA*

Estimated By:	Robert W. Fulp, PE (Engineer-of-Record)
Company:	Lincks & Associates, Inc.
Date:	7-Feb-25
Project No.:	22050 / F4

Roadway	\$	171,012	(1)
Signing / Pavement Markings	\$	15,000	(2)
Signalization & Lighting	\$	796,025	(1)
<b>Sub-Total</b>	\$	<b>982,037</b>	
Contingency Items (10%)	\$	98,204	(3)
Maintenance of Traffic (5%)	\$	49,102	
Mobilization (5%)	\$	49,102	
Bonding (3%)	\$	29,461	
MOT Plans	\$	3,500	(2)
Construction Surveying	\$	5,000	(2)
Permits	\$	2,000	(2)
Material Testing	\$	5,000	(2)
As-Builts	\$	3,500	(2)
<b>Sub-Total</b>	\$	<b>1,226,906</b>	
Independent Contractors Fee (8%)	\$	98,152	
<b>Grand Total</b>	\$	<b>1,325,058</b>	

### Notes:

(1) The estimate was based on FDOT's current 12-month (12/1/23 - 11/31/24) moving average unit costs for Area 7, which includes Hernando County. See attached plans.

(2) This is an estimated value based on the size of the project.

(3) Due to project unknowns, such as unexpected utility relocations, etc., a contingency of 10% was assumed.

**Exhibit “E”**

**Transportation Analysis**



February 10, 2025

Cliff Manuel  
Coastal Engineering  
966 Candlelight Boulevard  
Brooksville, FL 34601

Re: Project Name: US 19/Glen Lakes Boulevard  
Lincks Project No: 22050

Dear Mr. Manuel,

The purpose of this letter is to provide the proportionate share cost for the signalization of the intersection of US 19 and Glen Lakes Boulevard. The following methodology was utilized to calculate the proportionate share:

1. The following formula was utilized to determine the proportionate share of the required improvements.

Proportionate Share – (Project Traffic/LOS D Capacity) x (Roadway Cost)

2. The cost of the improvements are based on the cost estimate dated 1/27/25 – See Appendix.
3. The project traffic was obtained from the Access Management Analysis conducted for the project dated April, 2024.
4. The proportionate share percentage was calculated as shown in Table 1-A included in the appendix of this letter.

Table 1 provides the proportionate share cost which was calculated to be \$349,020.

It should be noted that the proposed development located north of Glen Lakes Boulevard is not anticipated to add much if any additional trips to the subject intersection for the following reasons:

1. The project will have access to the median opening at the Publix Shopping Center.

5023 West Laurel Street  
Tampa, FL 33607  
813 289 0039 Telephone  
813 287 0674 Telefax  
[www.lincks.com](http://www.lincks.com) Website

TABLE 1  
INTERSECTION PROPORTIONATE SHARE DETERMINATION

<u>Intersection</u>	<u>Improvement</u>	<u>Improvement Cost (1)</u>	<u>Percentage of LOS D Capacity Consumed by Project Traffic (2)</u>	<u>Proportionate Share</u>
US 19 and Glen Lakes Blvd	Signal	\$1,325,058	26.34%	\$349,020

(1) Source: FDOT Roadway Cost Per Centerline Mile, Revised August, 2023.

(2) % LOS D Consumed-See Table A-1 in the Appendix.



2. The project would also have access to the intersection of US 19 and Bourassa Boulevard that is to be signalized.
3. Finally, if the project traffic for the proposed development goes to the Publix Shopping Center then to Glen Lakes Boulevard it would be included in the Publix Shopping Center traffic.

If you have any questions, please do not hesitate to contact me at [shenry@lincks.com](mailto:shenry@lincks.com) or 813-559-9589.

LINCKS & ASSOCIATES, LLC  
A TMC Company

Steven Henry, PE  
President



## APPENDIX



## ACCESS MANAGEMENT ANALYSIS



# *ACCESS MANAGEMENT ANALYSIS*

## *GLEN LAKES COMMONS*

*Prepared For*

*GLEN LAKES COMMONS, LLC.*

*Prepared By*



*LINCKS & ASSOCIATES, INC.*

*Engineers - Planners*

*Tampa, Florida*



# ACCESS MANAGEMENT ANALYSIS

## GLEN LAKES COMMONS

Prepared For

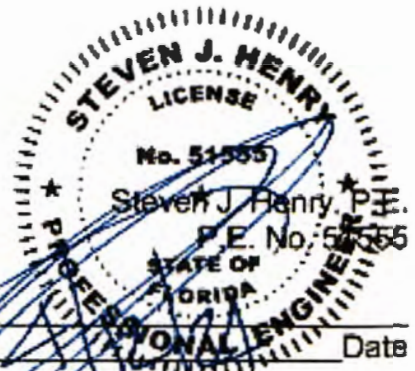
GLEN LAKES COMMONS, LLC.

Prepared By

LINCKS & ASSOCIATES, INC.  
5023 West Laurel Street  
Tampa, Florida 33607  
813-289-0039  
State of Florida Authorization No. EB0004638

Revised April, 2024  
November, 2023

Project No. 22050



LINCKS & ASSOCIATES, INC.

## INTRODUCTION

The purpose of this report is to provide an Access Management Analysis for the proposed development located south of Glen Lakes Boulevard and west of US 19 in Hernando County, Florida, as shown in Figure 1. The subject property is proposed to be developed for the following land uses:

- Assisted Living – 210 Beds
- Hotel – 80 Rooms
- Mini-Warehouse – 100,000 Square Feet
- Fast Food Restaurant w/Drive Thru – 3,000 Square Feet
- Fast Food Restaurant w/Drive Thru – 4,500 Square Feet
- Convenience Store/Gas Station – 5,200 Square Feet / 12 Fueling Positions
- Fast Food Restaurant w/Drive Thru – 3,500 Square Feet
- Medical-Dental Office Building – 20,000 Square Feet

The access for the project is proposed to be as follows:

- One (1) full access to Glen Lakes Boulevard to align with Outer Banks Road
- One (1) right-in/right-out access to US 19

This report will evaluate the access to serve the project. A copy of the site plan is included in the Appendix of this report.

## ESTIMATED DAILY PROJECT TRAFFIC

The trip rates utilized in this report were obtained from the latest computerized version of





TABLE 1  
ESTIMATED DAILY PROJECT TRIP ENDS

<u>Land Use</u>	<u>ITE LUC</u>	<u>Size</u>	<u>Daily Trip Ends (1)</u>	<u>Passerby Capture (2)</u>	<u>New Daily Trip Ends</u>
Assisted Living	254	210 Beds	546	0	546
Hotel	310	80 Rooms	639	0	639
Mini-Warehouse	151	100,000 SF	145	0	145
Fast Food Restaurant w/Drive Thru	934	3,000 SF	1,402	382	1,020
Fast Food Restaurant w/Drive Thru	934	4,500 SF	2,104	573	1,531
Convenience Store/Gas Station	945	5,200 SF	3,642	1,487	2,155
Fast Food Restaurant w/Drive Thru	934	3,500 SF	1,636	445	1,191
Medical-Dental Office Building	720	20,000 SF	<u>720</u>	<u>0</u>	<u>720</u>
		Total	10,834	2,887	7,947

(1) Source - ITE Trip Generation Manual, 11<sup>th</sup> Edition, 2021.

(2) Source - ITE Trip Generation Manual, 11<sup>th</sup> Edition, 2021.

• Passerby Percentage:

Fast Food Restaurant w/Drive Thru - 50%

(3,000 SF)  $1,402 \times 0.50 = 701$

(4,500 SF)  $2,104 \times 0.50 = 1,052$

(3,500 SF)  $1,636 \times 0.50 = 818$

Convenience Store/Gas Station - 75%

$3,642 \times 0.75 = 2,732$

• Passerby should not exceed 10% of the adjacent street traffic.

$28,867(a) \times 0.10 = 2,887 < 5,303$

(a) Based on PM peak hour background traffic at the intersection of US 19  
and Glen Lakes Boulevard and  $K = 0.09$ .

• Adjust passerby trips to 2,887 trip ends.





TABLE 2

## ESTIMATED AM PEAK HOUR PROJECT TRIP ENDS

Land Use	ITE LUC	Size	AM Peak Hour Trip Ends (1)			Passerby Capture (2)			New AM Peak Hour Trip Ends		
			In	Out	Total	In	Out	Total	In	Out	Total
Assisted Living	254	210 Beds	23	15	38	0	0	0	23	15	38
Hotel	310	80 Rooms	18	15	33	0	0	0	18	15	33
Mini-Warehouse	151	100,000 SF	5	4	9	0	0	0	5	4	9
Fast Food Restaurant w/Drive Thru	934	3,000 SF	68	66	134	14	14	28	54	52	106
Fast Food Restaurant w/Drive Thru	934	4,500 SF	103	98	201	22	20	42	81	78	159
Convenience Store/Gas Station	945	12 FP	162	162	324	50	50	100	112	112	224
Fast Food Restaurant w/Drive Thru	934	3,500 SF	80	76	156	17	16	33	63	60	123
Medical-Dental Office Building	720	20,000 SF	<u>45</u>	<u>12</u>	<u>57</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>45</u>	<u>12</u>	<u>57</u>
Total			504	448	952	103	100	203	401	348	749

(1) Source - ITE Trip Generation Manual, 11<sup>th</sup> Edition, 2021.

(2) Source - ITE Trip Generation Manual, 11<sup>th</sup> Edition, 2021.

• Passerby Percentage:

Fast Food Restaurant w/Drive Thru - 50%

(3,000 SF)

In:  $68 \times 0.50 = 34$

Out:  $66 \times 0.50 = 33$

(4,500 SF)

In:  $103 \times 0.50 = 52$

Out:  $98 \times 0.50 = 49$

(3,500 SF)

In:  $80 \times 0.50 = 40$

Out:  $76 \times 0.50 = 38$

Convenience Store/Gas Station - 76%

In:  $162 \times 0.76 = 123$

Out:  $162 \times 0.76 = 123$

• Passerby should not exceed 10% of the AM peak hour Background traffic.

$2,031(a) \times 0.10 = 203 < 492$

(a) AM peak hour background traffic at the intersection of US 19 and Glen Lakes Boulevard.

• Adjust passerby trips to 203.





TABLE 3

## ESTIMATED PM PEAK HOUR PROJECT TRIP ENDS

Land Use	ITE LUC	Size	PM Peak Hour Trip Ends (1)			Passerby Capture (2)			New PM Peak Hour Trip Ends		
			In	Out	Total	In	Out	Total	In	Out	Total
Assisted Living	254	210 Beds	20	30	50	0	0	0	20	30	50
Hotel	310	80 Rooms	16	15	31	0	0	0	16	15	31
Mini-Warehouse	151	100,000 SF	7	8	15	0	0	0	7	8	15
Fast Food Restaurant w/Drive Thru	934	3,000 SF	51	48	99	18	16	34	33	32	65
Fast Food Restaurant w/Drive Thru	934	4,500 SF	77	72	149	26	25	51	51	47	98
Convenience Store/Gas Station	945	5,200 SF	142	142	284	67	67	134	75	75	150
Fast Food Restaurant w/Drive Thru	934	3,500 SF	60	56	116	21	20	41	39	36	75
Medical-Dental Office Building	720	20,000 SF	<u>23</u>	<u>55</u>	<u>78</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>23</u>	<u>55</u>	<u>78</u>
Total			396	426	822	132	128	260	264	298	562

(1) Source - ITE Trip Generation Manual, 11<sup>th</sup> Edition, 2021.

(2) Source - ITE Trip Generation Manual, 11<sup>th</sup> Edition, 2021.

• Passerby Percentage:

Fast Food Restaurant w/Drive Thru - 55%

(3,000 SF)

In:  $51 \times 0.55 = 28$

Out:  $48 \times 0.55 = 26$

(4,500 SF)

In:  $77 \times 0.55 = 42$

Out:  $72 \times 0.55 = 40$

(3,500 SF)

In:  $60 \times 0.55 = 33$

Out:  $56 \times 0.55 = 31$

Convenience Store/Gas Station - 75%

In:  $142 \times 0.75 = 106$

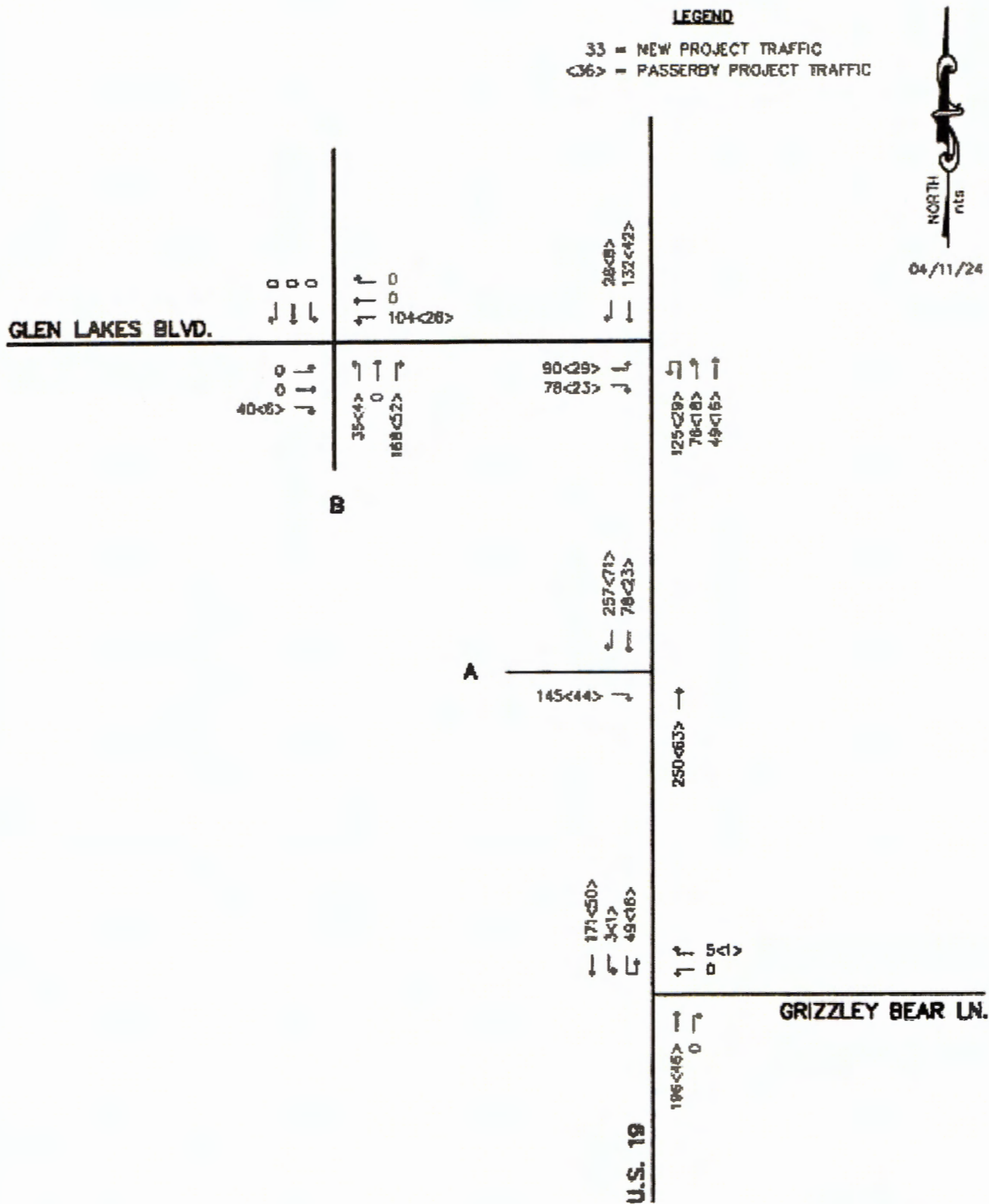
Out:  $142 \times 0.75 = 106$

• Passerby should not exceed 10% of the PM peak hour Background traffic.

$2,598(a) \times 0.10 = 260 < 412$

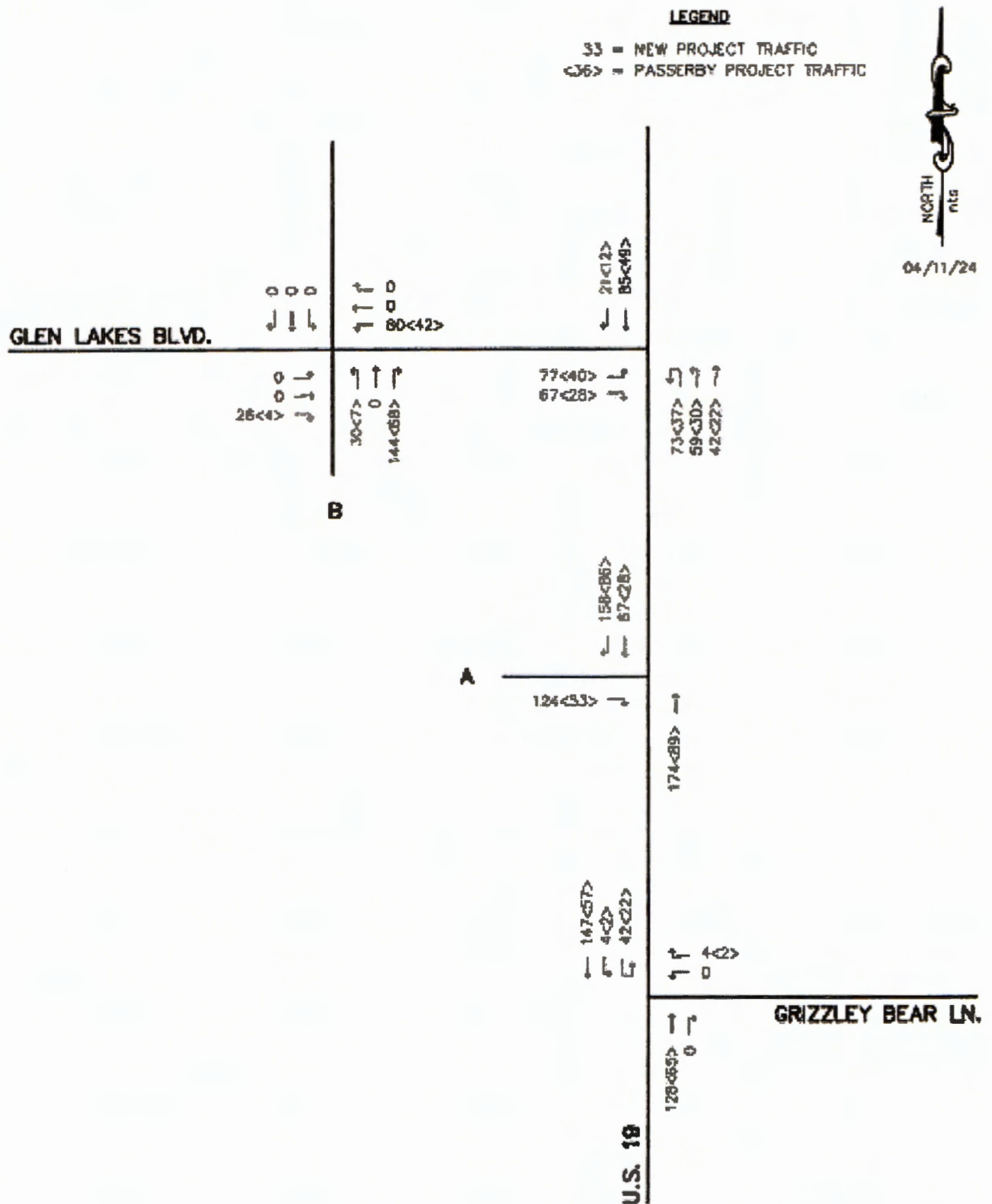
(a) PM peak hour background traffic at the intersection of US 19 and Glen Lakes Boulevard.

• Adjust passerby trips to 260.



**FIGURE 2**  
**AM PEAK HOUR**  
**PROJECT TRAFFIC**



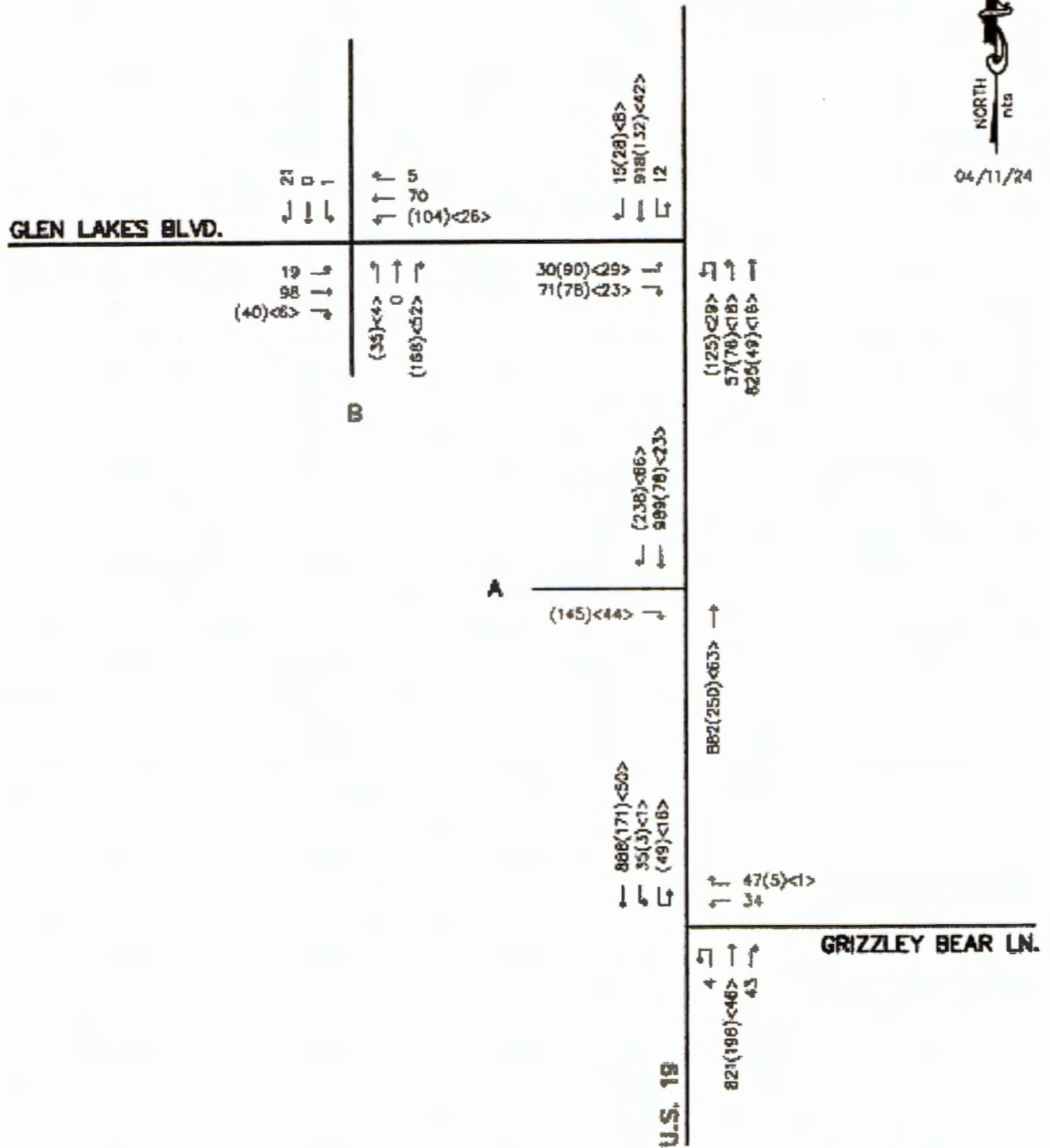


**FIGURE 3**  
**PM PEAK HOUR**  
**PROJECT TRAFFIC**



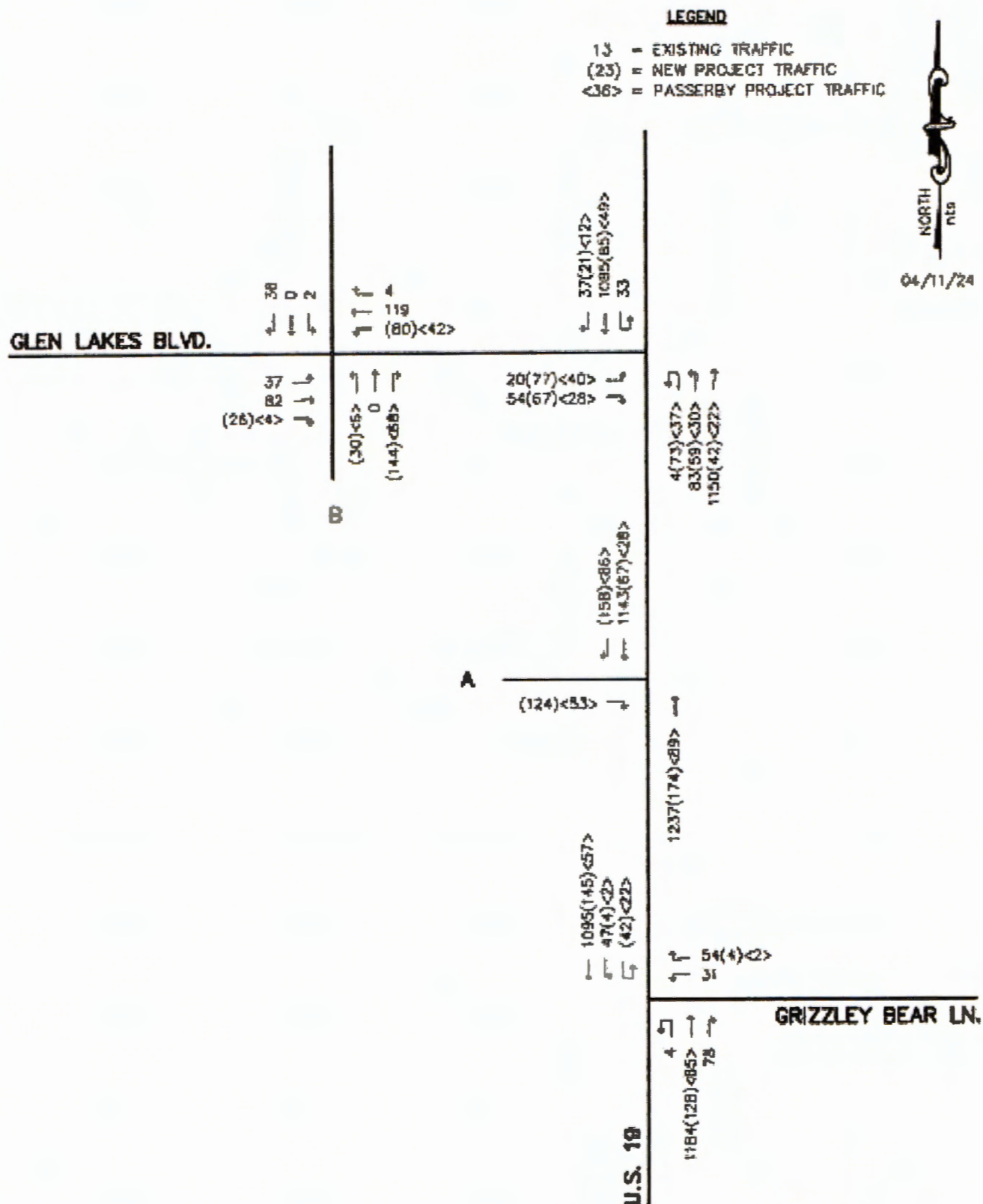


13 = EXISTING TRAFFIC  
(23) = NEW PROJECT TRAFFIC  
<36> = PASSERBY PROJECT TRAFFIC



**FIGURE 7  
AM PEAK HOUR  
BACKGROUND PLUS  
PROJECT TRAFFIC**





# Timings

1: US 19 & Glen Lakes Blvd

04/11/2024



Lane Group	EBL	EBR	NBL	NBT	SBU	SBT	SBR
Lane Configurations	←	→	←	↑↑	←	↑↑	→
Traffic Volume (vph)	149	172	305	890	12	1092	51
Future Volume (vph)	149	172	305	890	12	1092	51
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles (%)	2%	8%	4%	6%	2%	5%	8%
Shared Lane Traffic (%)							
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	pm+ov
Protected Phases	4	5	5	2		6	4
Permitted Phases		4	2		6		6
Detector Phase	4	5	5	2	6	6	4
Switch Phase							
Minimum Initial (s)	5.0	5.0	5.0	7.0	7.0	7.0	5.0
Minimum Split (s)	12.5	10.5	10.5	14.5	14.5	14.5	12.5
Total Split (s)	30.0	40.0	40.0	110.0	70.0	70.0	30.0
Total Split (%)	21.4%	28.6%	28.6%	78.6%	50.0%	50.0%	21.4%
Yellow Time (s)	5.5	3.5	3.5	5.5	5.5	5.5	5.5
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Lead/Lag		Lead	Lead		Lag	Lag	
Lead-Lag Optimize?		Yes	Yes		Yes	Yes	
Recall Mode	None	C-Max	C-Max	C-Max	None	None	None
Act Effct Green (s)	18.1	64.5	108.9	106.9	62.5	62.5	88.1
Actuated g/C Ratio	0.13	0.46	0.78	0.76	0.45	0.45	0.63
v/c Ratio	0.74	0.28	0.62	0.39	0.06	0.81	0.06
Control Delay	77.6	21.4	32.6	6.4	23.1	38.7	2.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	77.6	21.4	32.6	6.4	23.1	38.7	2.2
LOS	E	C	C	A	C	D	A
Approach Delay	47.5			13.1		37.0	
Approach LOS	D			B		D	

## Intersection Summary

Cycle Length: 140

Actuated Cycle Length: 140

Offset: 0 (0%), Referenced to phase 2:NBT and 5:NBL, Start of Green

Natural Cycle: 80

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.81

Intersection Signal Delay: 27.5

Intersection LOS: C

Intersection Capacity Utilization 72.4%

ICU Level of Service C

Analysis Period (min) 15

Splits and Phases: 1: US 19 & Glen Lakes Blvd



















# HCM Signalized Intersection Capacity Analysis

1: US 19 & Glen Lakes Blvd

04/11/2024

							
Movement	EBL	EBR	NBL	NBT	SBU	SBT	SBR
Lane Configurations							
Traffic Volume (vph)	149	172	305	890	12	1092	51
Future Volume (vph)	149	172	305	890	12	1092	51
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Lane Util. Factor	1.00	1.00	1.00	0.95	1.00	0.95	1.00
Frt	1.00	0.85	1.00	1.00	1.00	1.00	0.85
Flt Protected	0.95	1.00	0.95	1.00	0.95	1.00	1.00
Satd. Flow (prot)	1770	1495	1736	3406	1770	3438	1495
Flt Permitted	0.95	1.00	0.09	1.00	0.28	1.00	1.00
Satd. Flow (perm)	1770	1495	157	3406	528	3438	1495
Peak-hour factor, PHF	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Adj. Flow (vph)	169	195	347	1011	14	1241	58
RTOR Reduction (vph)	0	15	0	0	0	0	25
Lane Group Flow (vph)	169	180	347	1011	14	1241	33
Heavy Vehicles (%)	2%	8%	4%	6%	2%	5%	8%
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	pm+ov
Protected Phases	4	5	5	2		6	4
Permitted Phases		4	2		6		6
Actuated Green, G (s)	18.1	57.0	106.9	106.9	62.5	62.5	80.6
Effective Green, g (s)	18.1	57.0	106.9	106.9	62.5	62.5	80.6
Actuated g/C Ratio	0.13	0.41	0.76	0.76	0.45	0.45	0.58
Clearance Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	228	608	558	2600	235	1534	940
v/s Ratio Prot	c0.10	0.08	c0.17	0.30		c0.36	0.00
v/s Ratio Perm		0.04	0.30		0.03		0.02
v/c Ratio	0.74	0.30	0.62	0.39	0.06	0.81	0.04
Uniform Delay, d1	58.7	28.0	30.8	5.6	22.0	33.6	12.9
Progression Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Incremental Delay, d2	12.2	0.3	5.1	0.4	0.1	3.3	0.0
Delay (s)	70.9	28.3	36.0	6.0	22.1	36.8	12.9
Level of Service	E	C	D	A	C	D	B
Approach Delay (s)	48.1			13.7		35.6	
Approach LOS	D			B		D	
<b>Intersection Summary</b>							
HCM 2000 Control Delay		27.3		HCM 2000 Level of Service		C	
HCM 2000 Volume to Capacity ratio		0.74					
Actuated Cycle Length (s)		140.0		Sum of lost time (s)		20.5	
Intersection Capacity Utilization		72.4%		ICU Level of Service		C	
Analysis Period (min)		15					
c Critical Lane Group							



# Timings

## 1: US 19 & Glen Lakes Blvd

04/11/2024



Lane Group	EBL	EBR	NBL	NBT	SBU	SBT	SBR
Lane Configurations	←	→	←	→	←	→	←
Traffic Volume (vph)	137	149	286	1214	33	1219	70
Future Volume (vph)	137	149	286	1214	33	1219	70
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Heavy Vehicles (%)	2%	9%	2%	3%	2%	5%	6%
Shared Lane Traffic (%)							
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	Perm
Protected Phases	4	5	5	2		6	
Permitted Phases		4	2		6		6
Detector Phase	4	5	5	2	6	6	6
Switch Phase							
Minimum Initial (s)	5.0	5.0	5.0	7.0	7.0	7.0	7.0
Minimum Split (s)	12.5	10.5	10.5	14.5	14.5	14.5	14.5
Total Split (s)	30.0	45.0	45.0	110.0	65.0	65.0	65.0
Total Split (%)	21.4%	32.1%	32.1%	78.6%	46.4%	46.4%	46.4%
Yellow Time (s)	5.5	3.5	3.5	5.5	5.5	5.5	5.5
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Lead/Lag		Lag	Lag		Lead	Lead	Lead
Lead-Lag Optimize?		Yes	Yes		Yes	Yes	Yes
Recall Mode	None	None	None	C-Min	C-Min	C-Min	C-Min
Act Effct Green (s)	16.3	50.3	110.7	108.7	76.7	76.7	76.7
Actuated g/C Ratio	0.12	0.36	0.79	0.78	0.55	0.55	0.55
v/c Ratio	0.68	0.28	0.58	0.46	0.17	0.66	0.08
Control Delay	75.3	27.3	27.9	6.4	22.9	26.4	4.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	75.3	27.3	27.9	6.4	22.9	26.4	4.7
LOS	E	C	C	A	C	C	A
Approach Delay	50.3			10.5		25.2	
Approach LOS	D			B		C	

### Intersection Summary

Cycle Length: 140

Actuated Cycle Length: 140

Offset: 0 (0%), Referenced to phase 2:NBTU and 6:SBTU, Start of Green

Natural Cycle: 65

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.68

Intersection Signal Delay: 20.4

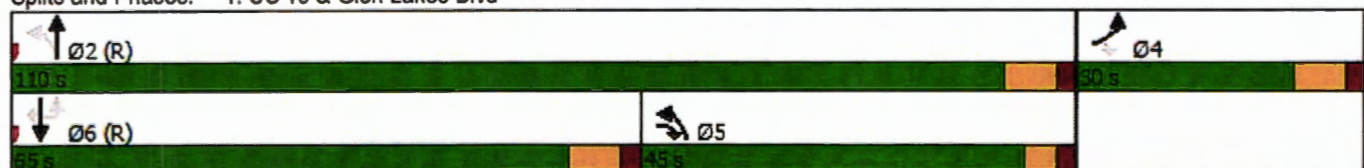
Intersection LOS: C

Intersection Capacity Utilization 74.2%

ICU Level of Service D

Analysis Period (min) 15

Splits and Phases: 1: US 19 & Glen Lakes Blvd



















# HCM Signalized Intersection Capacity Analysis

## 1: US 19 & Glen Lakes Blvd

04/11/2024

							
Movement	EBL	EBR	NBL	NBT	SBU	SBT	SBR
Lane Configurations							
Traffic Volume (vph)	137	149	286	1214	33	1219	70
Future Volume (vph)	137	149	286	1214	33	1219	70
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Lane Util. Factor	1.00	1.00	1.00	0.95	1.00	0.95	1.00
Frt	1.00	0.85	1.00	1.00	1.00	1.00	0.85
Flt Protected	0.95	1.00	0.95	1.00	0.95	1.00	1.00
Satd. Flow (prot)	1770	1482	1770	3505	1770	3438	1524
Flt Permitted	0.95	1.00	0.15	1.00	0.20	1.00	1.00
Satd. Flow (perm)	1770	1482	276	3505	364	3438	1524
Peak-hour factor, PHF	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Adj. Flow (vph)	140	152	292	1239	34	1244	71
RTOR Reduction (vph)	0	12	0	0	0	0	32
Lane Group Flow (vph)	140	140	292	1239	34	1244	39
Heavy Vehicles (%)	2%	9%	2%	3%	2%	5%	6%
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	Perm
Protected Phases	4	5	5	2		6	
Permitted Phases		4	2		6		6
Actuated Green, G (s)	16.3	42.8	110.7	108.7	76.7	76.7	76.7
Effective Green, g (s)	16.3	42.8	110.7	108.7	76.7	76.7	76.7
Actuated g/C Ratio	0.12	0.31	0.79	0.78	0.55	0.55	0.55
Clearance Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	206	511	501	2721	199	1883	834
v/s Ratio Prot	c0.08	0.05	c0.11	0.35		c0.36	
v/s Ratio Perm		0.04	0.35		0.09		0.03
v/c Ratio	0.68	0.27	0.58	0.46	0.17	0.66	0.05
Uniform Delay, d1	59.3	36.8	24.5	5.4	15.8	22.4	14.7
Progression Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Incremental Delay, d2	8.6	0.3	1.7	0.6	1.9	1.8	0.1
Delay (s)	68.0	37.1	26.2	6.0	17.6	24.3	14.8
Level of Service	E	D	C	A	B	C	B
Approach Delay (s)	51.9			9.8		23.6	
Approach LOS	D			A		C	
<b>Intersection Summary</b>							
HCM 2000 Control Delay			19.6		HCM 2000 Level of Service		B
HCM 2000 Volume to Capacity ratio			0.65				
Actuated Cycle Length (s)			140.0		Sum of lost time (s)		20.5
Intersection Capacity Utilization			74.2%		ICU Level of Service		D
Analysis Period (min)			15				
c Critical Lane Group							

## COST ESTIMATE



# ENGINEER'S CONSTRUCTION COST ESTIMATE

## SIGNALIZATION PROJECT

*U.S. 19 @ GLEN LAKES BOULEVARD*

*HERNANDO COUNTY, FLORIDA*

Estimated By:	Robert W. Fulp, PE (Engineer-of-Record)
Company:	Lincks & Associates, Inc.
Date:	7-Feb-25
Project No.:	22050 / F4

Roadway	\$	171,012	(1)
Signing / Pavement Markings	\$	15,000	(2)
Signalization & Lighting	\$	796,025	(1)
 Sub-Total	 \$	 982,037	
 Contingency Items (10%)	 \$	 98,204	 (3)
 Maintenance of Traffic (5%)	 \$	 49,102	
 Mobilization (5%)	 \$	 49,102	
 Bonding (3%)	 \$	 29,461	
 MOT Plans	 \$	 3,500	 (2)
 Construction Surveying	 \$	 5,000	 (2)
 Permits	 \$	 2,000	 (2)
 Material Testing	 \$	 5,000	 (2)
 As-Builts	 \$	 3,500	 (2)
 <b>Sub-Total</b>	 \$	 <b>1,226,906</b>	
 Independent Contractors Fee (8%)	 \$	 98,152	
 <b>Grand Total</b>	 \$	 <b>1,325,058</b>	

### Notes:

(1) The estimate was based on FDOT's current 12-month (12/1/23 - 11/31/24) moving average unit costs for Area 7, which includes Hernando County. See attached plans.

(2) This is an estimated value based on the size of the project.

(3) Due to project unknowns, such as unexpected utility relocations, etc., a contingency of 10% was assumed.

PROPORTIONATE SHARE CALCULATION





TABLE A-1  
PROPORTIONATE SHARE CALCULATION

<u>Intersection</u>	<u>Time Period</u>	<u>Critical Movement</u>	<u>LOS E Lane Group Capacity</u>	<u>LOS D Lane Group Capacity (1)</u>	<u>New Project Trips</u>
US 19 and Glen Lakes Blvd	AM	EB Left	228	202	119
		NB Left	558	494	248
		SB Through	1,534	<u>1,358</u>	<u>174</u>
				2,054	541
				% Consumed	26.34%
	PM	EB Left	206	182	117
		NB Left	501	443	199
		SB Through	1,883	<u>1,666</u>	<u>134</u>
				2,291	450
				% Consumed	19.64%

(1) Adjusted by 0.885