

GLEN LAKE'S COMMONS
SIGNALIZED INTERSECTION CONSTRUCTION AGREEMENT

THIS AGREEMENT (this "Agreement") is made and entered into this 8th day of April, 2025, by and between the Hernando County, a political subdivision of the State of Florida ("County") and Glen Lake's Commons, LLC ("GLC"). The County and GLC shall each be referred to herein as a "Party" and shall be collectively referred to herein as the "Parties."

RECITALS

WHEREAS GLC is the developer of the approximate 21.38-acre property described as Glen Lake's Commons commercial development ("GLC Project", as further described in Exhibit "A"); and

WHEREAS, the County has planned a to-be-named county frontage road connecting from the south to the north Glen Lakes Boulevard and Bourassa Boulevard along the west side of U.S. Highway 19, as approved by the Hernando County engineer; and

WHEREAS, GLC desires to cooperatively to assist the County in constructing the Signalized Intersection Project (as defined below) as specifically provided for in this Agreement; and

WHEREAS, the Parties desire to enter into this Agreement in order to provide for the rights and responsibilities of the Parties regarding the design, permitting, construction and funding of the Signalized Intersection Project.

NOW, THEREFORE, in consideration of the covenants hereinafter contained, and other good and valuable consideration, the receipt of which is hereby acknowledged, the Parties agree as follows:

AGREEMENT

1. **Recitals.** The above referenced recitals are true and correct and are incorporated herein by reference.
2. **Signalized Intersection Relocation Project Plans and Specifications.** The Parties plan for a signalized intersection as depicted in Exhibit "B". The Parties agree to the plans and design specifications for the Signalized Intersection Project which are contained in the "Construction Plans for the U.S. Highway 19 and Glen Lakes Blvd Intersection Improvements" prepared by Lincks & Associates, Inc. ("Lincks") and attached hereto as **Composite Exhibit "C" ("Plans and Specifications")**. Any material modifications to the Plans and Specifications shall require the approval of the Parties, which approval may be confirmed in a separate written instrument, including electronic format, by each Party. The County agrees that any approval for such modifications may be administratively approved and delivered by the Director of Public Works/County Engineer.
3. **The County's Role – Project Manager.** The Parties agree that the County shall be the project manager responsible for the design, permitting, construction, and funding of the Signalized Intersection Relocation Project as provided in this Agreement. Within ten (10) business days of the Effective Date of this Agreement, the County will coordinate with the Florida Department of Transportation ("FDOT") to assume the role of applicant and permittee with respect to the required FDOT application for construction, and take all necessary actions to obtain FDOT's approval of the Plans and Specifications, together with any other required permits to effectuate the construction

of the Signalized Intersection Project including the finalization and execution by the County of the FDOT Construction Agreement (collectively, “Government Authorizations”). The County shall use reasonable efforts to obtain all required Governmental Authorizations for the Signalized Intersection Project within ninety (90) calendar days of the Effective Date of this Agreement. Any modifications to the Plans and Specifications must be approved by the Parties as set forth in Paragraph 2 above.

4. **Project General Contractor, Engineer, and Bidding Process for Final Section of Construction Contractor.** The Parties agree that GLC shall be the independent contractor responsible for the management of the construction of the Signalized Intersection Project, and Lincks shall continue to serve as the engineer. GLC agrees to solicit sealed alternate competitive bids to determine the Final Project Costs (defined below) for the Signalized Intersection Project and select the construction contractor. The final selection of the construction contractor shall be based upon the lowest reasonable and responsive alternate competitive bid. GLC shall reasonably attempt to obtain at least three (3) competitive bids, if possibly. GLC agrees to otherwise substantially comply with the County’s bid process. Each bid shall clearly indicate the cost of each component of the Signalized Intersection Project. GLC also agrees County personnel may, but are not obligated to, be present when the sealed bids are opened. GLC further agrees to allow the County to concurrently approve and accept the most reasonable and responsive low bid, and to approve contract documents and related payment terms prepared by GLC. The bid award for the Signalized Intersection Project will require Hernando County Commission approval. The County shall be responsible for entering into all agreements with the construction contractor, Lincks and other professionals/consultants performing work on the Signalized Intersection Project and, subject to Paragraph 6 below, shall be the responsible party for paying all contractors, professionals and consultants directly.
5. **Estimated and Final Project Costs.** Attached as **Exhibit “D”** is an estimate in the amount of \$1,325,058.00 from Lincks which represents the design, permitting and construction costs for the Signalized Intersection Project (“**Estimated Project Costs**”). The final Signalized Intersection Project costs shall be the total costs for the Signalized Intersection Project, including without limitation design, permitting and construction costs, upon the final completion of the Signalized Intersection Project on the date that the County obtains approval of the final Signalized Intersection Project as-built from all applicable governmental permitting entities, including FDOT and the County (“**Final Project Costs**”).
6. **Cost-Sharing.** Attached as **Exhibit “E”** is the transportation analysis prepared by Lincks dated January 29, 2025 (“**Transportation Analysis**”) which provides a proportionate share allocation for the Signalized Intersection Project. The Transportation Analysis assigns Seventy-Three and Sixty-Six Hundredths Percent (73.66%) of the Final Project Costs to the County which equates to \$976,037.72, based on the Estimated Project Costs, and Twenty-Six and Thirty-Four Hundredths Percent (26.34%) of the Final Project Costs to GLC, which equates to \$349,020.28, based on the Estimated Project Costs (the “**GLC Estimated Contribution**”). The Parties agree that the Final Project Costs shall be paid as follows:
 - a. Following the Effective Date and during the construction of the Signalized Intersection Project, GLC shall submit paid invoices for any and all out-of-pocket expenses related to the Signalized Intersection Project, including without limitation fees and costs paid to Lincks and other approved “soft costs” set forth in Exhibit “D” (the “**GLC Expenses**”). The GLC Expenses shall be credited toward the GLC Estimated Contribution.

Courtesy copy to: Spencer Fane LLP
201 North Franklin Street
Suite 2150
Tampa, Florida 33602
Attn: Derek Larsen-Chaney
Fax: (813) 405-8904
E-mail: dlarsenchaney@spencerfane.com

If to the County: Hernando County
15470 Flight Path Drive
Brooksville, FL 34604
Attn: County Administrator
Fax (352) 754-4477
Email: administration@co.hernando.fl.us

Courtesy copy to: Hernando County Attorney's Office
20 N. Main St., Ste. 462
Brooksville, FL 34601
Attn: Garth Coller, Esq.
Fax: (352) 754-4001
E-mail: CAO@hernandocounty.us

The failure by any party to deliver a courtesy copy as referenced above shall not constitute a default under the terms of this Agreement nor shall it create a defect in any notice which is otherwise properly given. Furthermore, it is agreed that, if any party hereto is represented by legal counsel, such legal counsel is authorized to deliver written notice directly to the other party on behalf of his or her client, and the same shall be deemed proper notice hereunder if delivered in the manner hereinabove specified. Any party hereto may, at any time by giving ten (10) business days written notice to the other party hereto, designate any other address in substitution of the foregoing address to which such notice shall be given and other parties to whom copies of all notices hereunder shall be sent.

11. **Miscellaneous.**

- a. **Participation.** Each of the Parties have participated fully in the negotiation and preparation of this Agreement with full benefit of counsel. Accordingly, this Agreement shall not be more strictly construed against any of the Parties, and shall be interpreted as if the Parties hereto jointly prepared it.
- b. **Cooperation.** The Parties shall execute in good faith such other and further documents as may be required to effectuate the terms of this Agreement.
- c. **Force Majeure.** If the performance of any obligation under this Agreement is prevented, restricted, or interfered with by reason of war, revolution, civil commotion, acts of public enemies, pandemic, epidemic, blockade, embargo, strikes, acts of God, floods, tropical storms or hurricanes, then the affected party shall, upon giving prior written notice to the other party, be excused from such performance to the extent of such prevention, restriction, or interference, provided that the affected party shall use commercially reasonable efforts to avoid or remove such causes of nonperformance, and shall continue performance hereunder with reasonable dispatch whenever such causes are removed.

- d. **Attorneys' Fees/Venue.** In the event of any dispute, litigation, or other proceeding between the Parties arising out of this Agreement, to enforce any provision of this Agreement, or any right of the Parties hereunder, each party to such dispute, litigation, or other proceeding shall pay its own attorney fees, costs and expenses incurred in court, at trial, on appeal, and in any other proceeding irrespective of whether a party prevails in such litigation or proceeding. In the event of litigation, venue shall be in Hernando County, Florida. The provisions of this Section shall survive termination of this Agreement.
- e. **Relationship of the Parties; No Third-Party Beneficiaries.** Nothing contained in this Agreement is intended to, or shall, or shall be deemed to, create a joint venture or partnership of any kind between the Parties hereto. Nothing contained in this Agreement is intended to create any rights in third parties.
- f. **Waiver of Jury Trial.** THE PARTIES JOINTLY AND SEVERALLY, HEREBY KNOWINGLY, VOLUNTARILY, AND INTENTIONALLY WAIVE THE RIGHT THEY MAY HAVE TO A TRIAL BY JURY IN RESPECT OF ANY LITIGATION BASED HEREON, OR ARISING OUT OF, UNDER OR IN CONNECTION WITH THIS AGREEMENT AND ANY AGREEMENT CONTEMPLATED TO BE EXECUTED IN CONJUNCTION HEREWITH, OR ANY COURSE OF CONDUCT, COURSE OF DEALING, STATEMENTS, WHETHER VERBAL OR WRITTEN, OR ACTIONS OF EITHER PARTY.
- g. **Severability.** In the event any one or more provisions contained in this Agreement shall for any reason be held invalid, illegal, or unenforceable in any respect, such invalidity, illegality, or unenforceability shall not affect any other provision hereof and this Agreement shall be construed as if such invalid, illegal, or unenforceable provision had not been contained herein.
- h. **Assignment.** Assignment of this Agreement by any Party must be approved in writing by the other Parties, which approval shall not be unreasonably withheld.
- i. **Binding Effect.** This Agreement shall be binding upon and shall inure to the benefit of the successors and assigns of the respective Parties.
- j. **Authority.** If applicable, the entity officer or manager executing this Agreement certifies by acknowledgment of the signature below that s/he has been properly authorized to enter into this Agreement on behalf of, and binding with respect to, such entity.
- k. **Counterparts.** This Agreement may be executed in any number of counterparts, each of which when so executed and delivered shall be an original, but such counterparts shall together constitute but one and the same instrument.
- l. **Amendment.** This Agreement is the complete agreement of the Parties and may not be changed, orally, but only by instrument in writing signed by the Parties.
- m. **Headings.** Titles and captions to paragraphs and sections are inserted for convenience only, and in no way define, limit, extend or describe the scope or intent of this Agreement or the paragraphs, sections or provisions herein.

- n. **No Waiver.** Failure of any Party to exercise any right or power given hereunder, or to insist upon compliance by another Party with its obligations set forth herein, shall not constitute a waiver of any Party's right to demand strict compliance with the terms and provisions of this Agreement. Parties further agree that the waiver of a Party's breach or threatened breach of any obligations under this Agreement shall not be construed as a waiver of any subsequent breach by that Party.
- o. **Business Day.** Business day means any day other than Saturday or Sunday or a legal holiday.
- p. **Effective Date.** The "Effective Date" of this Agreement shall be the date that the last of the Parties execute this Agreement.

(Remainder of page intentionally left blank. Signature page(s) to follow.)

IN WITNESS WHEREOF, the Parties hereto have set their hands and seals as of the dates set forth below.

HERNANDO COUNTY

ATTEST:

ja
Hindi Prouse, Deputy Clerk
Douglas Chorvat Jr.
Clerk of the Circuit Court

Brian Hawkins
Brian Hawkins
Hernando County, Board Chairman

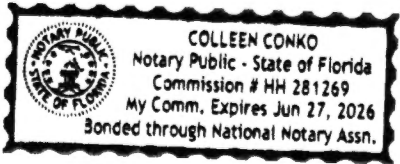


Date: 4-8-2025

**STATE OF FLORIDA
COUNTY OF HERNANDO**

The foregoing instrument was acknowledged before me by means of physical presence or online notarization, this 8th day of April, 2025, by Brian Hawkins as Chairman of the Hernando County Board of County Commissioners, who is personally known to me or who has produced _____ as identification.

[Notary Seal]



Colleen Conko
Notary Public
Colleen Conko
Name printed

My Commission Expires: 06-27-2026

FOR THE USE AND RELIANCE OF
HERNANDO COUNTY ONLY.
APPROVED AS TO FORM
AND LEGAL SUFFICIENCY.

Victoria Anderson
County Attorney's Office

WITNESSES:

Louiana D. Cantrell
Print Name: Alicana D. Cantrell
Address: 11173 Riley Pines Circle
Gibsonton, FL 33534

Elizabeth A. Coutu
Print Name: Elizabeth A. Coutu
Address: 7203 Meeting House Lane
Apollo Beach, FL 33572

GLEN LAKE'S COMMONS, LLC

[Signature]
By: _____
Name: ROBERT GOMEZ JR.
Title: MANAGING PARTNER
Date: 3/26/2025

STATE OF Florida
COUNTY OF Hillsborough

The foregoing instrument was acknowledged before me by means of physical presence or online notarization, this 21st day of March, 2025, by Robert Gomez, Jr. as Managing Partner of Glen Lake's Commons, LLC, a Florida limited liability company, who is personally known to me or who has produced _____ as identification.

[Notary



[Signature]
Notary Public
Lori N. Harris
Name printed
My Commission Expires: June 14, 2025

Exhibit "A"

LEGAL DESCRIPTION FOR GLEN LAKE'S COMMONS

THAT PART OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, TOWNSHIP 22 SOUTH, RANGE 17 EAST, HERNANDO COUNTY, FLORIDA, LYING WEST OF U.S. HIGHWAY #19.

BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHWEST CORNER OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, TOWNSHIP 22 SOUTH, RANGE 17 EAST, HERNANDO COUNTY, FLORIDA; THENCE ALONG THE WEST LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, RUN N01°18'06"E, 1330.05 FEET TO THE NORTH LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SAID SECTION 13; THENCE N50°41'32"W, 57.35 FEET TO A NON-TANGENT CURVE TO THE RIGHT AND THE EAST RIGHT-OF-WAY LINE OF NEW ORLEANS DRIVE, SAID CURVE BEING CONCAVE TO THE SOUTHEAST, HAVING A RADIUS OF 175.00 FEET, A CENTRAL ANGLE OF 31°34'08", AND A CHORD BEARING AND DISTANCE OF N23°39'40"E, 95.21 FEET; THENCE ALONG THE ARC OF SAID CURVE 96.42; THENCE N39°20'26"E, 238.56 FEET; THENCE LEAVING SAID EAST RIGHT-OF-WAY LINE, RUN S50°40'56"E, 63.11 FEET; THENCE S47°48'06"E, 62.58 FEET; THENCE S24°09'32"E, 93.48 FEET; THENCE S53°13'38"E, 43.57 FEET; THENCE S80°46'20"E, 82.26 FEET TO A NON-TANGENT CURVE TO THE LEFT, AND THE WEST RIGHT-OF-WAY LINE OF OUTER BANKS DRIVE, SAID CURVE BEING CONCAVE TO THE EAST, HAVING A RADIUS OF 353.95 FEET, A CENTRAL ANGLE OF 7°52'58", AND A CHORD BEARING AND DISTANCE OF S05°18'47"W, 48.66 FEET; THENCE ALONG THE ARC OF SAID CURVE 48.70 FEET; THENCE S01°10'27"W, 62.01 FEET TO THE NORTH LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SAID SECTION 13; THENCE ALONG SAID NORTH LINE, RUN S88°40'06"E, 49.97 FEET TO THE EAST RIGHT-OF-WAY LINE OF OUTER BANKS DRIVE; THENCE CONTINUE ALONG SAID NORTH LINE, RUN S88°42'23"E, 350.66 FEET TO THE WEST RIGHT-OF-WAY LINE OF U.S. HIGHWAY #19; THENCE ALONG SAID WEST LINE, RUN S14°08'08"W, 1365.17 FEET; THENCE LEAVING SAID WEST LINE ALONG THE SOUTH LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, RUN N88°33'43"W, 486.32 FEET TO THE POINT OF BEGINNING.

CONTAINING 21.38 ACRES MORE OR LESS.

Exhibit "B"

Depiction of Signalized Intersection



21093
SHEET
2

DATE	REV.	BY	REV. NO.	REVISION

REUSE OF DOCUMENT
THIS DOCUMENT, COMPRISED OF THE INCORPORATED IDEAS AND DESIGNS, A 3-D A.M. INSTRUMENT OF PROFESSIONAL SERVICE, IS THE PROPERTY OF COASTAL ENGINEERING ASSOCIATES, INC. AND IS NOT TO BE USED, IN WHOLE OR IN PART FOR ANY OTHER PURPOSE WITHOUT THE WRITTEN AUTHORIZATION OF COASTAL ENGINEERING ASSOCIATES, INC.

Coastal Engineering
Planning
Surveying
Environmental
Transportation
Construction Management
Engineering Associates, Inc.
988 Candlelight Boulevard - Brooksville - Florida 34601
(352) 796-9423 - Fax (352) 796-5358

ACCESS EXHIBIT

GLEN LAKES COMMONS

Composite Exhibit "C"
Plans and Specifications

GENERAL NOTES

- THE CONTRACTOR SHALL MEET THE CRITERIA OUTLINED IN FDOT'S "THE MINIMUM SPECIFICATIONS FOR TRAFFIC CONTROL SIGNAL DEVICES" (MSTCSB).
- THE LOCATIONS OF UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE. PRIOR TO DIGGING, THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATIONS OF ALL UTILITIES.
- ONE WEEK PRIOR TO THE START OF THE TRAFFIC SIGNAL INSTALLATION, THE SIGNAL CONTRACTOR SHALL CONTACT:
 - a) DWAYNE DEMPSEY
F.D.O.T. DISTRICT SEVEN TRAFFIC OPERATION CONSTRUCTION LIAISON
FLORIDA DEPARTMENT OF TRANSPORTATION
11201 NORTH MCINTLEY DRIVE
TAMPA, FLORIDA 33612
PHONE: (813) 975-6289
E-MAIL: curtis.dempsey@dot.state.fl.us
 - b) J. SCOTT HERRING, P.E.
HERNANDO COUNTY DIRECTOR OF PUBLIC WORKS AND COUNTY ENGINEER
1525 EAST JEFFERSON STREET
BROOKSVILLE, FLORIDA 34601
PHONE: (352) 754-4082
- PRIOR TO ALL TRAFFIC SIGNAL CHANGES (I.E. MAINTENANCE OF TRAFFIC CHANGES, NEW SIGNAL INSTALLATIONS, DIFFERENT PAVEMENT CONFIGURATIONS, ETC.) CONTACT RICK NAPORA, F.D.O.T. DISTRICT SEVEN TRAFFIC OPERATIONS, AT (813) 615-8620, AT LEAST 48 HOURS IN ADVANCE FOR IMPLEMENTATION OF APPROPRIATE SIGNAL TIMINGS.
- THE CONTRACTOR SHALL CONTACT THE LOCAL POWER COMPANY FOR THEIR ASSISTANCE IN PERFORMING ALL NECESSARY WORK UNDER POWER LINES. SUCH WORK SHALL INCLUDE, BUT NOT BE LIMITED TO THE INSTALLATION OF SPAN WIRE, SIGNAL CABLE, FIBERGLASS INSULATORS, AND / OR SIGNAL POLES
- UNLESS OTHERWISE NOTED, ALL REMOVED EQUIPMENT SHALL BE TURNED OVER TO THE HERNANDO COUNTY SIGNAL SHOP, IN EXISTING CONDITION, EXCEPT FOR POLES AND SPAN WIRES, WHICH SHALL BE DISPOSED OF BY THE CONTRACTOR IN AREAS PROVIDED BY HIM.
- THE CONTRACTOR SHALL SUBMIT THE AS-BUILT PLANS TO THE ENGINEER FOR APPROVAL PRIOR TO THE 48 HOUR TEST. UPON THE 48 HOUR TEST, THE ENGINEER SHALL PROVIDE AT LEAST ONE COPY OF THE FINAL AS-BUILT PLANS TO HERNANDO COUNTY AND ONE COPY TO DISTRICT TRAFFIC OPERATIONS
- THE CONTRACTOR SHALL FIELD VERIFY ALL CRITICAL ELEVATIONS PRIOR TO ORDERING SIGNAL STRAIN POLES AND MAST ARMS.
- THE TYPE OF EQUIPMENT USED IN THE INSTALLATION OF MAST ARMS/FOUNDATIONS, OVERHEAD/CANTILEVER SIGNS/FOUNDATIONS, AND THE MOVEMENT/INSTALLATION OF STRAIN POLES SHALL MEET THE FOLLOWING REQUIREMENTS:
 - a) OVERHEAD LINES SHALL STAY IN PLACE BOTH VERTICALLY AND HORIZONTALLY
 - b) CONTRACTOR SHALL MEET ALL APPLICABLE OSHA REQUIREMENTS. ANY COST ASSOCIATED WITH THE TYPE OF EQUIPMENT REQUIRED FOR THIS INSTALLATION IS INCLUDED IN THE RELATED PAY ITEMS.
- ALL GROUND ROD CONNECTIONS SHALL BE MADE USING EXOTHERMIC WELD GROUND CONNECTORS.
- ALL UNUSED REDS SHALL BE HARD WIRED TO 120V AC, UNLESS A FLORIDA DEPARTMENT OF TRANSPORTATION APPROVED PLUG-IN JUMPER IS USED.
- CONTACT DWAYNE DEMPSEY AT (813) 323-1159 WITH THE FDOT TRAFFIC OPERATIONS PRIOR TO PERFORMING THE 48-HOUR BURN-IN PERIOD TEST PER FDOT STANDARD SPECIFICATION 811-4.
- A MANDATORY PER-SIGNALIZATION MEETING SHALL BE HELD PRIOR TO START OF PROJECT. ALL MATERIAL SUBMITTALS SHALL BE PROVIDED AT THE MEETING OR PRIOR TO THE MEETING.
- PLEASE CONTACT JAMES MEYER 48 HOURS PRIOR TO WORKING IN FDOT RAW AT (352) 849-2610. TO ACTIVATE PERMIT AND SCHEDULE PRE-CONSTRUCTION MEETING.
- CONTRACTOR SHALL COMPLY WITH OSHA'S STANDARDS 29 CFR PART 1928, SUBPART CC FOR VERTICAL AND HORIZONTAL CLEARANCES TO THE OVERHEAD DISTRIBUTION AND TRANSMISSION POWER LINES.

PAY ITEM FOOTNOTES

- 630-2-11:
CONDUIT RUNS SHOWN ON THE PLANS ARE SCHEMATIC AND FIELD ADJUSTMENTS MAY BE NECESSARY WITH THE EXCEPTION OF ELECTRICAL POWER SERVICE DUCTS, AND JACK AND BORE SLEEVES, ALL UNDERGROUND AND UNDER PAVEMENT CONDUITS SHALL BE SCHEDULE 40 PVC WITH A MINIMUM SIZE OF TWO INCHES, UNLESS OTHERWISE SPECIFIED IN THE PLANS. TWO SEPARATE UNDERGROUND CONDUIT RUNS LOCATED 180 DEGREES APART ARE REQUIRED FOR ALL SIGNAL STRAIN POLES AND MAST ARMS. THERE SHALL BE MINIMUM OF TWO RUNS OF TWO INCH CONDUIT BETWEEN THE LAST LOW VOLTAGE (LOOPS) PULL BOX LOCATED NEAR THE CONTROLLER CABINET AND THE CONTROLLER CABINET.
- 630-2-12:
CONDUITS INSTALLED WITH THE DIRECTIONAL BORE METHOD SHALL BE HDPE WITH A MINIMUM SIZE OF TWO INCHES, UNLESS OTHERWISE NOTED IN THE PLANS.
ALL FOUR-INCH HDPE OUTERDUCT CONDUIT INSTALLED WITH DIRECTIONAL BORE METHOD SHALL INCLUDE THREE RUNS OF ONE INCH HDPE INNERDUCT CONDUIT. PAYMENT FOR FURNISHING AND INSTALLING THE INNERDUCT CONDUITS SHALL BE INCLUDED IN PAY ITEM 630-2-12.
- 632-7-1:
EACH PHASE/MOVEMENT SHALL BE WIRED FROM THE SIGNAL DISPLAY TO THE CONTROLLER AS A SEPARATE PHASE/MOVEMENT. THIS INCLUDES THE LEFT TURN MOVEMENT WHICH SHALL HAVE CONDUCTORS AVAILABLE FOR EITHER PROTECTED OR PERMISSIVE MOVEMENTS. THE CONTRACTOR SHALL VERIFY COLOR CODES FOR SIGNAL CABLE AND INTERCONNECT CABLE WITH THE HERNANDO COUNTY SIGNAL SHOP BEFORE ORDERING, AND WIRE THE SIGNAL IN ACCORDANCE WITH THAT COLOR CODE AND F.D.O.T. SPECIFICATIONS. THERE SHALL BE ONE NEUTRAL PER APPROACH.
- 632-7-1:
ALL TRAFFIC SIGNAL FIXTURES SHALL BE WIRED AND PHASED PER THE INTERSECTION WIRING DIAGRAM PROVIDED BY HERNANDO COUNTY TRAFFIC OPERATIONS. CABLE SIZE SHALL BE AS CALLED FOR IN THE WIRING DIAGRAM. ALL SIGNAL CABLE WIRING SHALL BE CONCEALED WITHIN THE ARM, MOUNTING BRACKET AND FIXTURE; NO EXPOSED WIRING SHALL BE PERMITTED. A MINIMUM OF THREE (3) FEET OF BLACK CABLE SHALL BE PROVIDED AT ALL HAND HOLES. ALL SIGNAL CABLES ON SPAN WIRES SHALL BE ATTACHED TO THE MESSANGER BY USE OF LASHING RODS IN PLACE OF THE WRAPS.
- 632-7-1:
ALL SPARE CONDUCTORS IN TRAFFIC SIGNAL FIXTURES SHALL BE CONNECTED TO AC IN THE FIXTURE. ALL SPARE CONDUCTORS IN THE CABINET SHALL BE CONNECTED TO THE GROUND BUSB BAR.
- 639-1-122:
THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE COMPANY PROVIDING ELECTRICAL POWER TO DETERMINE IF A SERVICE PROCESSING OR CONNECTION FEE IS REQUIRED. THE COST OF ANY SUCH FEE SHALL BE INCLUDED AS PART OF PAYMENT FOR ELECTRICAL POWER SERVICE ASSEMBLY.
- 639-1-122:
THE COST OF THIS ITEM SHALL INCLUDE THE COST FOR THE APPROPRIATE METER CAN CONTAINING AN INTERNAL BYPASS SHUNT SWITCH AS REQUIRED BY THE LOCAL POWER COMPANY. THE CONTRACTOR SHALL VERIFY THE TYPE OF METER CAN AND A BREAKER BOX TO USE WITH THE HERNANDO COUNTY SIGNAL SHOP. A SEPARATE CIRCUIT BREAKER SHALL BE ADDED TO THE SERVICE POLE FOR THE LUMINAIRES ATTACHED TO THE SIGNAL POLES. IN ADDITION, WIRE ALL LUMINAIRES TO ONE (1) PHOTO ELECTRIC CELL ATTACHED TO THE SERVICE POLE.
- 639-2-1:
THE PAYMENT OF THIS ITEM SHALL BE BASED ON THE LINEAR FOOT OF A COMPLETE WIRE (I.E. ALL CONDUCTORS).
- 649-1-11:
ALUMINUM PEDESTALS SHALL BE MOUNTED ON TRANSFORMER BASE.
- 649-21-21:
AT LOCATIONS WHERE UNDERGROUND UTILITIES ARE IN CLOSE PROXIMITY TO THE SIGNAL POLE FOUNDATION, AS DETERMINED BY THE CONTRACTOR, THE CONTRACTOR SHALL HAND DIG THE FIRST FOUR FEET OF THE HOLE FOR THE POLE FOUNDATION.
- 649-21-21:
ALL SPLICES IN THE POLE BASES SHALL BE MADE USING 3M 3576G CONNECTOR SEALING PACKS OR EQUAL. ALL SPLICES SHALL BE MADE AT THE HAND HOLE. SPLICES SHALL BE MADE PER HERNANDO COUNTY STANDARD SPLICING DETAILS. A MINIMUM OF THREE (3) FEET OF BLACK CABLE SHALL BE PROVIDED AT ALL HAND HOLES
- 649-21-21:
ALL SIGNAL POLE FOUNDATIONS SHALL HAVE A MINIMUM OF THREE 2" AND ONE 1" PVC CONDUITS. UNUSED CONDUITS SHALL BE RUN TO THE NEAREST SIGNAL PULL BOX OR SIGNAL CABINET, WHICHEVER IS CLOSEST.
- 653-1-11:
ALL WALK AND DON'T WALK PEDESTRIAN SIGNALS SHALL BE LED COUNT-DOWN PER FDOT SPECIFICATIONS WITH A SOLID SYMBOL DISPLAY.
- 660-4-11 & 660-4-12:
THIS ITEM SHALL INCLUDE ALL LABOR AND MATERIALS REQUIRED TO FURNISH AND INSTALL A VIDEO DETECTION SYSTEM. THE DETECTION SYSTEM SHALL BE COMPATIBLE WITH THE HERNANDO COUNTY'S CONTROLLER EQUIPMENT, CABINET AND TRAFFIC SIGNAL COMPUTER SYSTEM. THE CONTRACTOR SHALL COORDINATE WITH THE MAINTAINING AGENCY FOR UNIT ACCEPTANCE AND SHALL BE LISTED IN THE F.D.O.T. APPROVED PRODUCT LIST. THE VIDEO DETECTION SYSTEM SHALL BE VISION HD CAMERAS. CONTACT CONNIE BRAITHWAITE AT ECONOLITE CONTROL PRODUCTS: (904) 759-0745.

PAY ITEM FOOTNOTES

- 665-1-11:
SHALL BE A MECHANICAL TYPE PUSHBUTTON (NO LED) AND SHALL INCLUDE ADDITIONAL COST OF LABOR AND MATERIALS REQUIRED FOR INSTALLATION OF PEDESTRIAN SIGNAL SIGN FTP-68B-06.
- 670-5-111:
ALL FIELD WIRES IN CABINET SHALL BE IDENTIFIED WITH PANDUIT #PLF1M-C MARKER TIES, OR EQUIVALENT.
- 670-6-111:
ALL CONTROLLER CABINET BASES SHALL HAVE ONE 4-INCH CONDUIT FOR FUTURE FIBER OPTICS COMMUNICATIONS. IT SHALL BE STUBBED OUT TO A FIBER OPTICS PULL BOX.
- 670-6-111:
PRIOR TO SIGNAL CABINET ASSEMBLY INSTALLATION, THE SIGNAL CONTRACTOR SHALL VERIFY THE OPERATION OF THE SIGNAL EQUIPMENT BY PERFORMING A BURN-IN OF THE ASSEMBLY FOR EACH LOCATION WITHIN THE PROJECT. A REPRESENTATIVE FROM THE FDOT TRAFFIC OPERATIONS WILL OBSERVE AND ATTEST TO THE VERIFICATION. THIS BURN-IN WILL BE AT A DESIGNATED LOCATION SUPPLIED BY THE SIGNAL CONTRACTOR.
- 670-5-111:
ENSURE THAT ALL TRAFFIC SIGNAL CONTROLLERS ARE FULLY COMPATIBLE WITH THE HERNANDO COUNTY SIGNAL SYSTEM. THE COUNTY'S CURRENT SYSTEM COMPRISED OF COBALT CONTROLLER WITH 8MB DATA KEY AND RECEPTACLE. PROVIDE FDOT TYPE-V TRAFFIC SIGNAL CABINETS WITH A PHOTO-CELL, RACK MOUNT DETECTORS, ALL INTERFACE PANELS INCLUDING PEDESTRIAN ISOLATION PANEL, ETHERNET SWITCH AND 18 IN. ALUMINUM CABINET RISER (FROM CABINET MANUFACTURE), ALL CABLING NOT INCLUDING CONNECTION TO THE PATCH PANELS, NECESSARY TO CONNECT THE ETHERNET SWITCH TO ALL ITEMS INCLUDING THE UPS, VIDEO DETECTION AND CONFLICT MONITOR. THE UPS SHALL BE MOUNTED ON A SEPARATE CONCRETE SLAB. NOTIFY HERNANDO COUNTY SIGNAL SHOP AND FDOT DISTRICT-7 TRAFFIC OPERATIONS VIA PHONE AT LEAST 48 HOURS IN ADVANCE OF TURNING ON A NEW OR MODIFIED CONTROLLER CABINET ASSEMBLY. CABINET DOOR SHOULD OPEN AWAY FROM TRAFFIC. INCLUDE AN EIGHT PHASE BACKPLANE WITH MINIMUM OF 16 POSITIONS ON THE LOAD BAY IN THE CABINET. COORDINATE WITH HERNANDO COUNTY SIGNAL SHOP FOR ACCEPTABLE CHOICES OF TRAFFIC SIGNAL CONTROLLERS AND CONTROLLER CABINETS. INSTALL THE SIGNAL CABLES IN ITS OWN CONDUIT. HERNANDO COUNTY CURRENTLY UTILIZES ECONOLITE TS-2 TYPE-1 CONTROLLER CABINETS.
- 682-1-113:
THE CCTV CAMERA - CONTACT AND COORDINATE WITH HERNANDO COUNTY FOR ACCEPTABLE CHOICES OF CAMERAS.
- 684-1-1:
THE ETHERNET SWITCH SHALL BE A ITS 8012 WITH (4) 1000bSFP SINGLE MODE, LC, 1550 NM, 25KM TRANSCEIVERS. CONFIRM MAKE AND MODEL WITH HERNANDO COUNTY DEPARTMENT OF PUBLIC WORKS PRIOR TO SUBMITTING SHOP DRAWINGS. PAY ITEM TO INCLUDE ALL APPROPRIATE PATCH CORDS AND CABLING NECESSARY CONNECTIONS TO ALL EQUIPMENT INSIDE THE CONTROLLER CABINET.
- 685-1-13:
THE UNINTERRUPTED POWER SUPPLY (UPS) UNIT SHALL BE HOUSED IN A SEPARATE ENCLOSED CABINET OR IN A TRAFFIC SIGNAL CABINET WHICH IS DESIGNED TO HOUSE THE UPS ASSEMBLY IN ADDITION TO THE TRAFFIC SIGNAL CONTROL EQUIPMENT. IF A SEPARATE CABINET IS REQUIRED, THIS PAY ITEM SHALL INCLUDE CABINET TO HOUSE THE UPS ASSEMBLY. THE TRAFFIC SIGNAL CABINET CONCRETE BASE SHALL BE LARGE ENOUGH TO SUPPORT THE TRAFFIC SIGNAL CABINET AND THE UPS CABINET. PAYMENT FOR ADDITIONAL CONCRETE BASE, INCLUDING LABOR AND MATERIAL, SHALL BE INCLUDED IN TRAFFIC CONTROLLER ASSEMBLY PAY ITEM NUMBER. THE CONTRACTOR SHALL COORDINATE WITH DISTRICT SEVEN TRAFFIC OPERATIONS FOR SPECIFICATIONS. THE UPS UNIT SHALL BE OF THE SAME MAKE AND MODEL AS IS CURRENTLY BEING USED BY THE MAINTAINING AGENCY.
- 685-1-13:
THE TOP ELEVATION OF THE CONTROLLER CABINET BASE AND THE TECHNICIAN SERVICE PAD SHALL BE A MINIMUM OF 4 INCHES ABOVE GROUND FINISHED GRADE. THE BASE SHALL BE INTEGRATED WITH THE UPS AS A SINGLE BASE FOR MOUNTING OF BOTH THE CONTROLLER CABINET AND UPS. THE CONTROLLER CABINET SHALL HAVE AN EXTENSION RISER TO BRING BOTTOM OF THE CABINET TO THE CROWN IN THE ROAD. THE UPS REPLACEMENT SHALL BE COORDINATED WITH HERNANDO COUNTY TRAFFIC OPERATIONS. THERE SHALL BE A MINIMUM OF TWO (2) FEET SEPARATION BETWEEN THE CONTROLLER CABINET AND THE UPS CABINET.
- 685-1-13:
UPS - HOUSE AN ALPHA FX 1100 OR APPROVED EQUAL UNINTERRUPTED POWER SUPPLY UNIT IN AN ALUMINUM NEMA 4X ENCLOSURE MOUNTED ON A SEPARATE CONCRETE PAD. INCLUDE THE FOLLOWING 1100 WATT POWER MODULE WITH SNMP ETHERNET CONNECTION THAT MEETS UL 1778, FOUR BATTERIES AND GENERATOR CORD ACCESS COMPARTMENT. INSTALL ONE 2 IN. AND ONE 1 IN. CONDUITS TO THE TRAFFIC CABINET FOUNDATION. INSTALL A NETWORK CABLE THROUGH THE ONE IN. CONDUIT FOR REMOTE ACCESS. COORDINATE WITH HERNANDO COUNTY TRAFFIC OPERATIONS FOR SPECIFICATIONS PRIOR TO ORDERING.
- 700-3-201:
SHALL INCLUDE PROPERLY DESIGNED AND SIZED ADJUSTABLE HANGERS, BRACKETS, CLAMPS, AND ALL MISCELLANEOUS HARDWARE NECESSARY TO INSTALL THE SIGNS AS SHOWN IN THE PLANS.
- 700-5-22:
SHALL BE EDGE LIT L.E.D. TYPE AND SHALL BE LISTED IN THE F.D.O.T. APPROVED PRODUCT LIST.

LINKS & ASSOCIATES, LLC
5023 West Laurel Street
Tampa, Florida 33607
Tel: (813) 289-0097
Fax: (813) 289-0094
Lic. No. EB0004638



Engineers
Planners

DATE	08/15/24
DESIGNED	QAR/RWF
DRAWN	QAR
CHECKED	RWF

SIGNALIZATION PLANS
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

GENERAL NOTES

**60% PLANS
NOT FOR
CONSTRUCTION
12/20/24**

REVISIONS	
SHEET	T-3 OF T-

BROOKSVILLE MAINTENANCE FDOT GENERAL NOTES

ALL WORK PERFORMED IN THE DEPARTMENT'S RIGHT OF WAY SHALL BE DONE IN ACCORDANCE WITH THE MOST CURRENT EDITIONS OF:

- A) FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- B) FDOT DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM.
- C) FDOT DESIGN MANUAL (FDM)
- D) FDOT FLEXIBLE PAVEMENT DESIGN MANUAL FOR NEW CONSTRUCTION AND PAVEMENT REHABILITATION.

1) ALL AREAS IN THE FDOT RIGHT OF WAY DISTURBED DURING CONSTRUCTION SHALL BE RESTORED, COMPACTED, SODDED AND WATERED IN CONFORMANCE WITH THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 570 AND 981 AND DESIGN STANDARDS INDEX 570-010.

2) DAYTIME WORK ONLY. THE FDOT ENGINEER MUST APPROVE NIGHT WORK AND LANE CLOSURES AT LEAST 48 HOURS IN ADVANCE. THE APPLICANT MUST NOTIFY THE FLORIDA DEPARTMENT OF TRANSPORTATION LOCAL MAINTENANCE OFFICE 48 HOURS IN ADVANCE OF STARTING ANY PROPOSED WORK. CALL: BROOKSVILLE MAINTENANCE, James Meyer @ 352-848-2610

3) ALL TRAFFIC STRIPES AND PAVEMENT MARKINGS ARE TO BE LEAD-FREE, NON-SOLVENT BASED THERMOPLASTIC. THE PERMITTEE SHALL FURNISH THE DEPARTMENT WITH THE MANUFACTURER'S CERTIFICATION THAT THE THERMOPLASTIC IS 'LEAD FREE'. REMOVAL OF EXISTING STRIPING SHALL BE DONE BY MILLING AND RESURFACING OF THE FRICTION/SURFACE COURSE TO OBLITERATE OBSOLETE PAVEMENT MARKINGS.

4) ALL PROPOSED TRAFFIC SIGNS AND PAVEMENT MARKINGS SHALL BE IN CONFORMANCE WITH AND CONSTRUCTED TO THE FDOT DESIGN STANDARDS INDEX SERIES 600 AND 700 SERIES. THE PROPERTY OWNER MUST MAINTAIN THE TRAFFIC SIGNS AND MARKINGS FOR THE DRIVEWAY.

5) THE FDOT REQUIRES DOCUMENTATION FOR SUCCESSFUL COMPLETION OF AN APPROVED WORK ZONE TRAFFIC CONTROL TRAINING COURSE FOR THE AGENCY, UTILITY OR CONTRACTOR EMPLOYEE(S) INSTALLING AND/OR MAINTAINING THE APPROVED MAINTENANCE OF TRAFFIC PLAN. DOCUMENTATION IS TO BE FURNISHED TO THE DEPARTMENT AT THE PRE-CONSTRUCTION MEETING OR BEFORE OCCUPYING STATE RIGHT-OF-WAY.

6) MAINTENANCE OF TRAFFIC PLAN FOR WORK ZONES SHALL BE IN CONFORMANCE WITH ALL APPLICABLE INDICES OF THE FDOT DESIGN STANDARDS INDEX 102 SERIES ACCORDING TO THE TYPE OF ROADWAY AND TYPE OF WORK BEING PERFORMED.

7) SILT FENCE FOR EROSION CONTROL IN CONFORMANCE WITH THE FLORIDA EROSION SEDIMENT CONTROL MANUAL IS REQUIRED FOR ALL AREAS OF CONSTRUCTION WITHIN FDOT R/W, PERPENDICULAR TO AND PARALLEL WITH THE STATE ROAD.

8) SIDEWALK SHALL BE CONSTRUCTED PER FDOT INDEX 522-001 AND 522-002. DETECTABLE WARNING STRIPS, A.K.A. TRUNCATED DOMES, FOR DISTRICT SEVEN CONSTRUCTION AND MAINTENANCE SHOULD BE EITHER INSET CERAMIC TILES OR THERMOPLASTIC DETECTABLE WARNING STRIPS. THESE WARNING SURFACES SHALL ONLY BE PROVIDED BY THE FOLLOWING VENDORS/MANUFACTURERS.

- A) INLINE TRUNCATED DOME EZ TILE SUPPLIED BY PROFESSIONAL PAVEMENT PRODUCTS
- B) TOPMARK SUPPLIED BY FLINT TRADING
- C) VANGUARD TRUNCATED DOME SUPPLIED BY VANGUARD

9) MAINTENANCE OF TRAFFIC PLAN MUST INCLUDE FDOT DESIGN STANDARDS INDEX 102-880 PEDESTRIAN CONTROL FOR CLOSURE OF SIDEWALKS.

10) FDOT REQUIRES A PIPE VIDEO BE DONE ON ALL PIPES ASSOCIATED WITH A PERMIT PROJECT IN FDOT ROW. SPEC 430-4.8.

11) PROVIDE C.E.I. INSPECTION PER FDOT SPECIFICATION 105 "CONTRACTOR QUALITY CONTROL GENERAL REQUIREMENTS".

HENRY KLOBUCAR
406-498-8510
ZAYO GROUP / FORMERLY
LIGHTWAVE, LLC
130 N. MAIN ST.
BUTTE, MT 89701

ALLEN TURNER
352-549-8219
HERNANDO COUNTY
UTILITY DEPT.
15385 CORTEZ BLVD.
BROOKSVILLE, FL 34801

UTILITY / AGENCY OWNERS

NETWORK RELATIONS
877-388-8344 Ext: 2
CENTURY LINK
1025 ELDERADO BLVD.
BROOMFIELD, CO 80021

DINO FARRUGGIO
861-883-2729
AT&T / DISTRIBUTION
8628 LAKESIDE RD.
WEST PALM BEACH, FL 33411

DON TAULBEE
352-595-4000 Ext: 3136
WITHLACOCHEE RIVER ELECTRIC
COOPERATIVE
10006 CORTEZ BLVD.
WEEKI WACHEE, FL 34613

LINKS & ASSOCIATES, LLC

5023 West Laurel Street
Tampa, Florida 33607
Tel: (813) 288-0079
Fax: (813) 288-0074
Lic. No. EB0004638



DATE	08/15/24	DESIGNED	QAR/RWF	DRAWN	QAR	CHECKED	RWF
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SIGNALIZATION PLANS
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

GENERAL NOTES

PRIOR TO CONSTRUCTION
48 HOURS BEFORE DIGGING
CALL
TOLL FREE
1-800-432-4770
SUNSHINE STATE ONE CALL CENTER

60% PLANS
NOT FOR
CONSTRUCTION
12/20/24

REVISIONS	

LINCKS & ASSOCIATES, LLC
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Tampa, Florida 33607
Tel (813) 288-0038
Fax (813) 287-0674
Lic. No. EB0004638



DATE	09/19/24
DESIGNED	QAR/RMF
DRAWN	QAR
CHECKED	RWF

SIGNALIZATION PLANS
U.S. 19 (Commercial Way) & Glen Lakes Blvd.
GUIDE SIGN WORKSHEET
(ADVANCE STREET SIGNS)

REVISIONS

SHEET T-9 of T-

60% PLANS
NOT FOR
CONSTRUCTION
12/20/24

THIS SHEET NOT USED
(SEE ROADWAY PLANS)

TO BE COMPLETED
FOR FINAL SUBMITTAL

60% PLANS
NOT FOR
CONSTRUCTION
12/20/24

REVISIONS

SIGNALIZATION PLANS
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

REPORT OF
JOE BOBINO

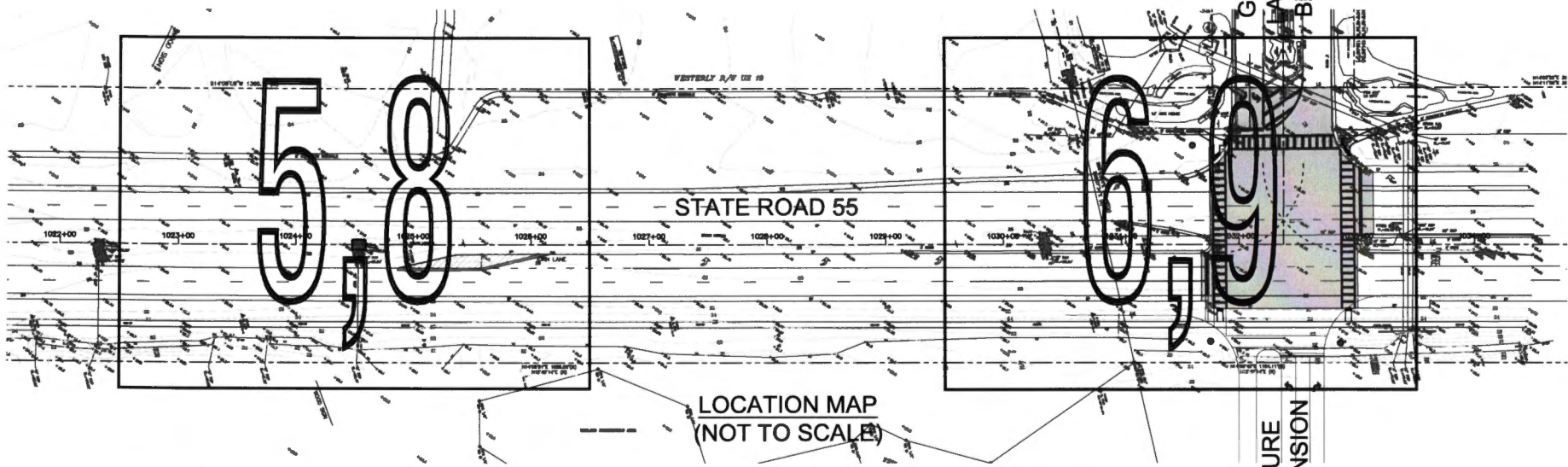
DATE	09/18/24
DESIGNED	
DRAWN	
CHECKED	

BY OTHERS

TO BE COMPLETED
FOR FINAL SUBMITTAL

60% PLANS
NOT FOR
CONSTRUCTION
12/20/24

SHEET T-14a T-	REVISIONS	SIGNALIZATION PLANS U.S. 19 (Commercial Way) & Glen Lakes Blvd.	DATE 08/19/24	BY OTHERS
		SUBSURFACE UTILITY	DESIGNED OAR/RWF	
		ENGINEERING DETAIL	DRAWN OAR	
			CHECKED RWF	

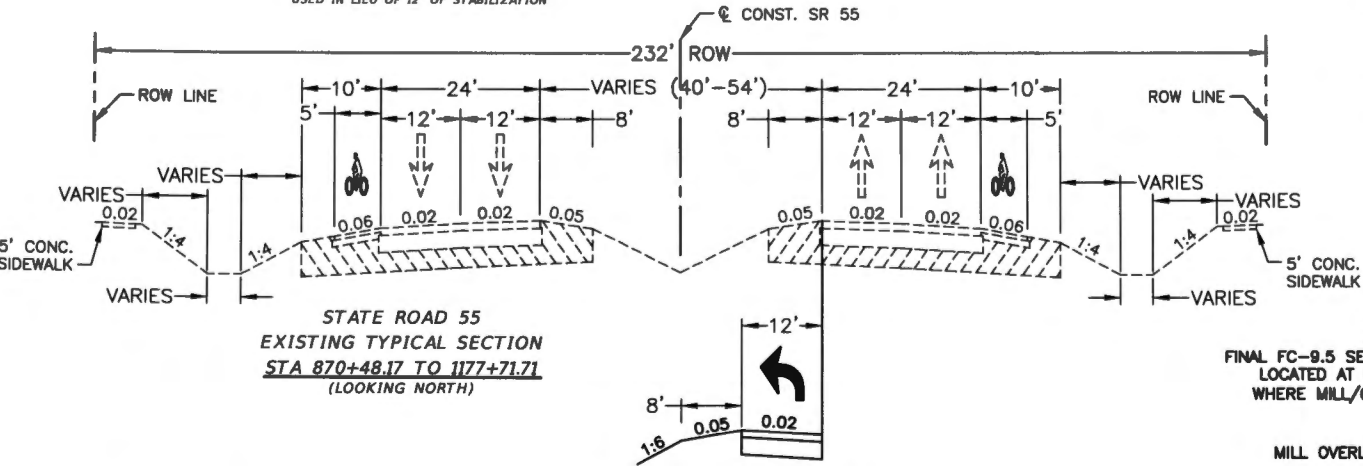


TURN LANE & WIDENING
 (SEE PLAN VIEW FOR LIMITS)
 FRICTION COURSE FC-9.5 (1.5") (PG 76-22)
 STRUCTURAL COURSE SP-9.5 (TRAFFIC C, PG-76-22) (3")
 BASE COURSE RECYCLED CONCRETE AGGREGATE 10" (LBR 150)
 12" TYPE "B" (LBR40) STABILIZATION

SHOULDER PAVING
 (SEE PLAN VIEW FOR LIMITS)
 FRICTION COURSE FC-9.5 (1.5") (PG 76-22)
 STRUCTURAL COURSE SP-9.5 (TRAFFIC D, PG-76-22) (1")
 OPTIONAL BASE GROUP 1
 12" TYPE "B" (LBR40) STABILIZATION

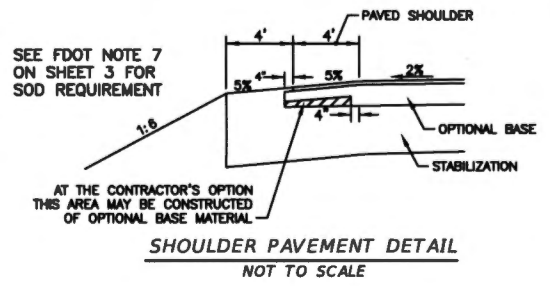
**MILL & OVERLAY
 (MATCH EXISTING SLOPE)**
 MILL 1.5" OF EXISTING ASPHALT
 OVERLAY WITH:
 FRICTION COURSE FC-9.5 (1.5") (PG-22)

NOTE: 6" OF ADDITIONAL BASE MAY BE USED IN LIEU OF 12" OF STABILIZATION

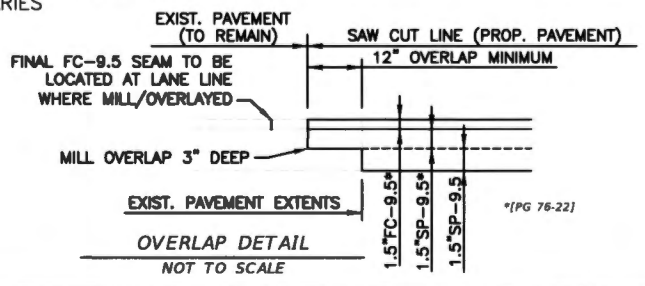


**STATE ROAD 55
 EXISTING TYPICAL SECTION
 STA 870+48.17 TO 1177+71.71
 (LOOKING NORTH)**

**LEFT TURN LANE EXTENSION
 STA 1024+86.5 TO 1025+58.2**



**SHOULDER PAVEMENT DETAIL
 NOT TO SCALE**



**OVERLAP DETAIL
 NOT TO SCALE**

**STATE ROAD 55 (US 19)
 POSTED SPEED LIMIT: 60 MPH
 DESIGN SPEED: 70 MPH**

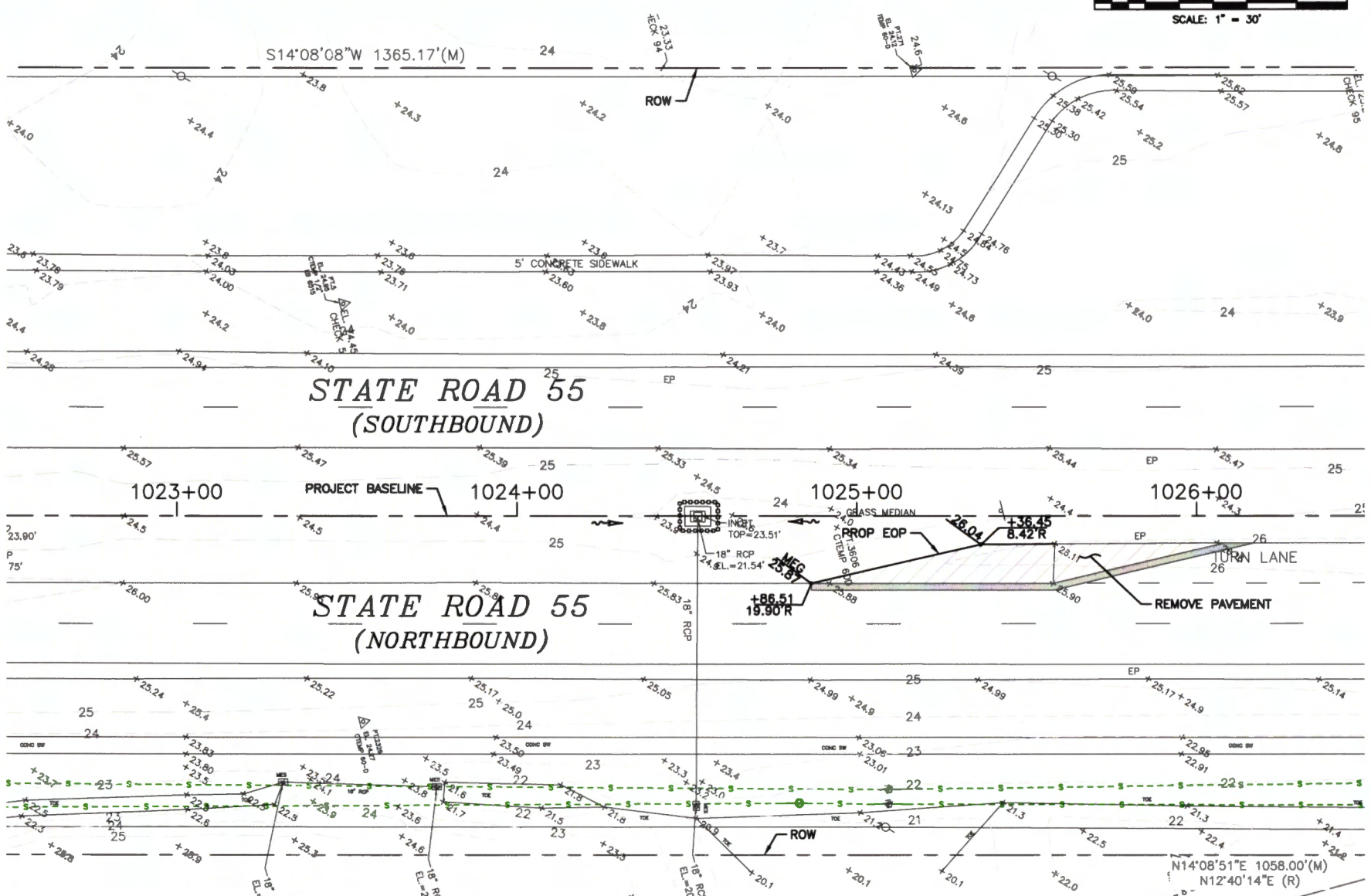
LINCKS & ASSOCIATES, LLC.
 5023 West Laurel Street
 Tampa, Florida 33607
 Tel: (813) 286-0039
 Fax: (813) 287-0874



DATE	12/20/24
DESIGNED	JPA
DRAWN	JPA
CHECKED	DWD

**SR 55 Roadway Improvements
 Glen Lakes Blvd
 KEY SHEET AND**

REVISIONS	
SHEET	2



PAVING LEGEND	
	PROPOSED ROAD PAVING
	PROPOSED SHOULDER PAVING
	PROPOSED MILL & OVERLAY

STATE ROAD 55 (US 19)
POSTED SPEED LIMIT: 60 MPH
DESIGN SPEED: 70 MPH

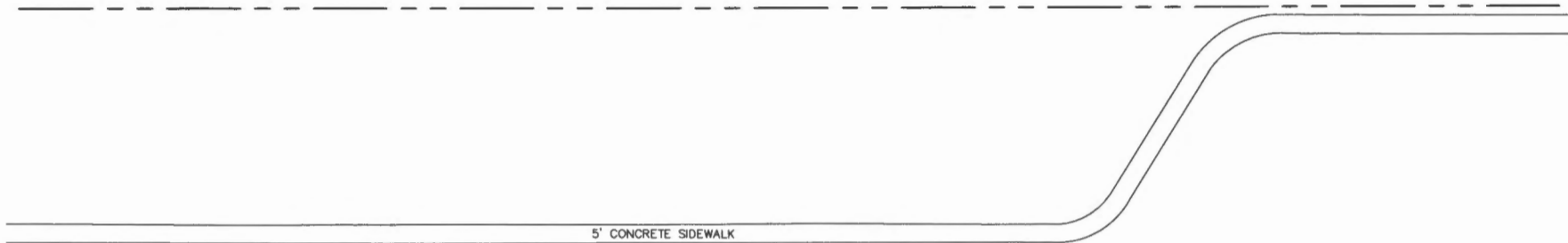
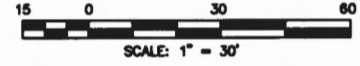
LINKS & ASSOCIATES, LLC.
 9023 West Laurel Street
 Tampa, Florida 33607
 Tel: (813) 289-0039
 Fax: (813) 287-1174



DATE	12/20/24
DESIGNED	JPA
DRAWN	JPA
CHECKED	DWD

SR 55 Roadway Improvements
Glen Lakes Blvd
ROADWAY PLAN

NO.	DATE	DESCRIPTION



STATE ROAD 55
(SOUTHBOUND)

1023+00

1024+00

1025+00
GRASS MEDIAN

1026+00

STATE ROAD 55
(NORTHBOUND)



LINCKS & ASSOCIATES, LLC.
5023 West Loral Street
Tampa, Florida 33607
Tel (813) 288-0038
Fax (813) 287-1874

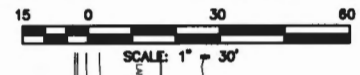
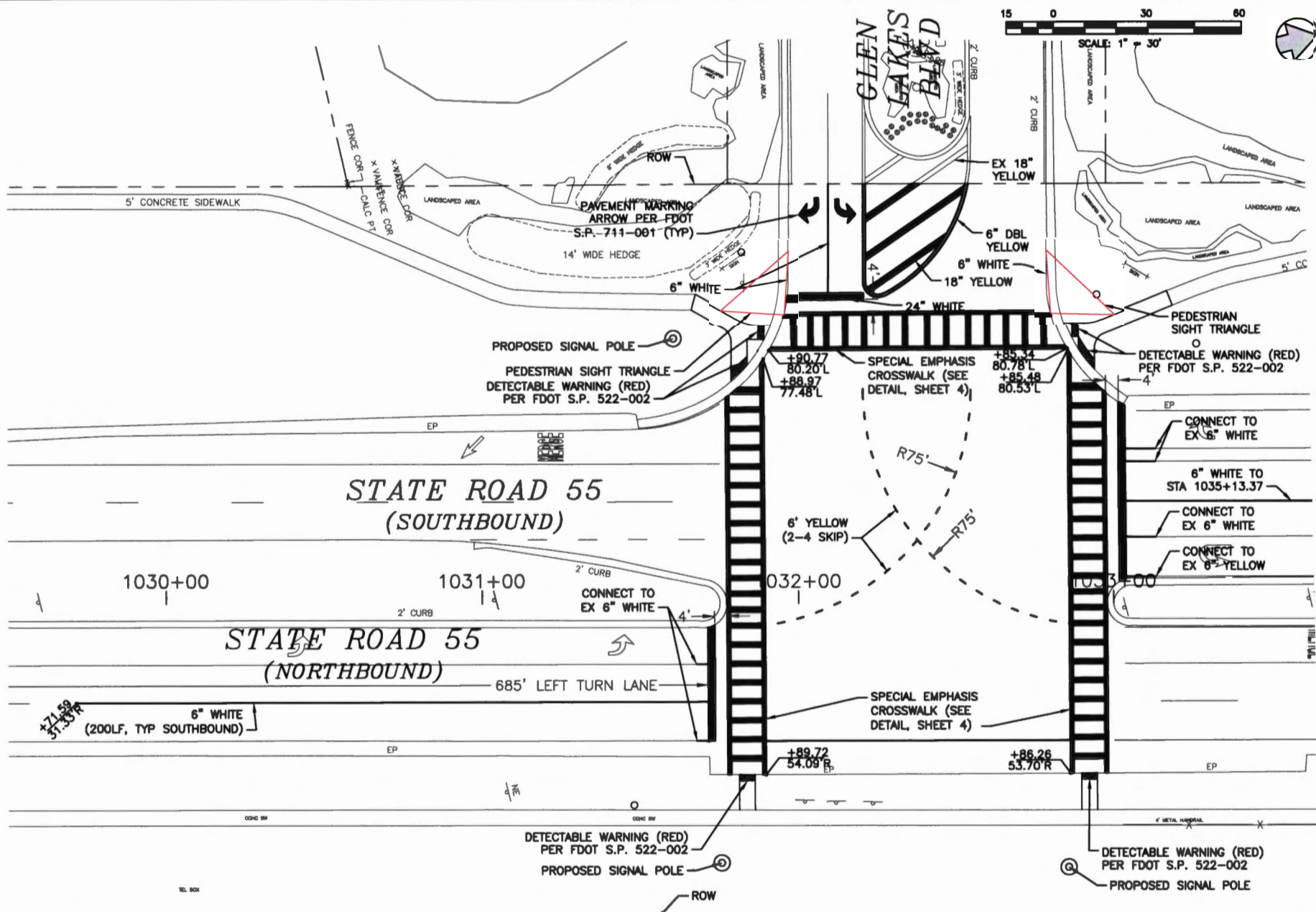


DATE	12/20/24
DESIGNED	JPA
DRAWN	JPA
CHECKED	DWD

**SR 55 Roadway Improvements
Glen Lakes Blvd**
SIGNING AND PAVEMENT

REVISIONS

STATE ROAD 55 (US 19)
POSTED SPEED LIMIT: 60 MPH
DESIGN SPEED: 70 MPH



LINCKS & ASSOCIATES, LLC.
 9023 West Laurel Street
 Tampa, Florida 33607
 Tel (813) 288-0038
 Fax (813) 287-1874



DATE	12/20/24
DESIGNED	JPA
DRAWN	JPA
CHECKED	DWD

**SR 55 Roadway Improvements
 Glen Lakes Blvd
 SIGNING AND PAVEMENT**

REVISIONS	

STATE ROAD 55 (US 19)
POSTED SPEED LIMIT: 60 MPH
DESIGN SPEED: 70 MPH

Exhibit "D"

Estimated Project Costs

ENGINEER'S CONSTRUCTION COST ESTIMATE

SIGNALIZATION PROJECT U.S. 19 @ GLEN LAKES BOULEVARD HERNANDO COUNTY, FLORIDA

Estimated By: Robert W. Fulp, PE (Engineer-of-Record)
Company: Lincks & Associates, Inc.
Date: 7-Feb-25
Project No.: 22050 / F4

Roadway	\$	171,012	(1)
Signing / Pavement Markings	\$	15,000	(2)
Signalization & Lighting	\$	796,025	(1)
Sub-Total	\$	982,037	
Contingency Items (10%)	\$	98,204	(3)
Maintenance of Traffic (5%)	\$	49,102	
Mobilization (5%)	\$	49,102	
Bonding (3%)	\$	29,461	
MOT Plans	\$	3,500	(2)
Construction Surveying	\$	5,000	(2)
Permits	\$	2,000	(2)
Material Testing	\$	5,000	(2)
As-Builts	\$	3,500	(2)
Sub-Total	\$	1,226,906	
Independent Contractors Fee (8%)	\$	98,152	
Grand Total	\$	1,325,058	

Notes:

(1) The estimate was based on FDOT's current 12-month (12/1/23 - 11/31/24) moving average unit costs for Area 7, which includes Hernando County. See attached plans.

(2) This is an estimated value based on the size of the project.

(3) Due to project unknowns, such as unexpected utility relocations, etc., a contingency of 10% was assumed.

Exhibit "E"

Transportation Analysis



February 10, 2025

Cliff Manuel
Coastal Engineering
966 Candlelight Boulevard
Brooksville, FL 34601

Re: Project Name: US 19/Glen Lakes Boulevard
Lincks Project No: 22050

Dear Mr. Manuel,

The purpose of this letter is to provide the proportionate share cost for the signalization of the intersection of US 19 and Glen Lakes Boulevard. The following methodology was utilized to calculate the proportionate share:

1. The following formula was utilized to determine the proportionate share of the required improvements.

Proportionate Share – (Project Traffic/LOS D Capacity) x (Roadway Cost)

2. The cost of the improvements are based on the cost estimate dated 1/27/25 – See Appendix.
3. The project traffic was obtained from the Access Management Analysis conducted for the project dated April, 2024.
4. The proportionate share percentage was calculated as shown in Table 1-A included in the appendix of this letter.

Table 1 provides the proportionate share cost which was calculated to be \$349,020.

It should be noted that the proposed development located north of Glen Lakes Boulevard is not anticipated to add much if any additional trips to the subject intersection for the following reasons:

1. The project will have access to the median opening at the Publix Shopping Center.

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Tampa, FL 33607
813 289 0039 Telephone
8133 287 0674 Telefax
www.Lincks.com Website

TABLE 1

INTERSECTION PROPORTIONATE SHARE DETERMINATION

<u>Intersection</u>	<u>Improvement</u>	<u>Improvement Cost (1)</u>	<u>Percentage of LOS D Capacity Consumed by Project Traffic (2)</u>	<u>Proportionate Share</u>
US 19 and Glen Lakes Blvd	Signal	\$1,325,058	26.34%	\$349,020

(1) Source: FDOT Roadway Cost Per Centerline Mile, Revised August, 2023.

(2) % LOS D Consumed-See Table A-1 in the Appendix.

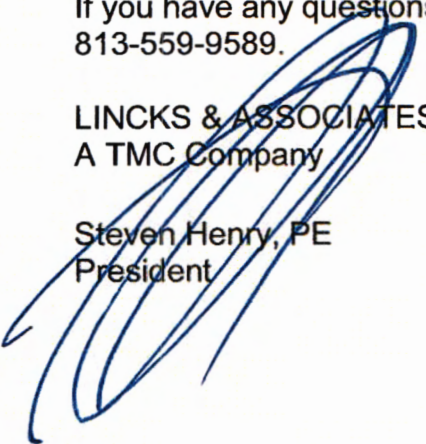
Mr. Cliff Manuel
February 10, 2025
Page 3

2. The project would also have access to the intersection of US 19 and Bourassa Boulevard that is to be signalized.
3. Finally, if the project traffic for the proposed development goes to the Publix Shopping Center then to Glen Lakes Boulevard it would be included in the Publix Shopping Center traffic.

If you have any questions, please do not hesitate to contact me at shenry@lincks.com or 813-559-9589.

LINCKS & ASSOCIATES, LLC
A TMC Company

Steven Henry, PE
President



APPENDIX



ACCESS MANAGEMENT ANALYSIS



ACCESS MANAGEMENT ANALYSIS

GLEN LAKES COMMONS

Prepared For

GLEN LAKES COMMONS, LLC.

Prepared By



LINCKS & ASSOCIATES, INC.

Engineers - Planners

Tampa, Florida

ACCESS MANAGEMENT ANALYSIS

GLEN LAKES COMMONS

Prepared For

GLEN LAKES COMMONS, LLC.

Prepared By

LINCKS & ASSOCIATES, INC.

5023 West Laurel Street

Tampa, Florida 33607

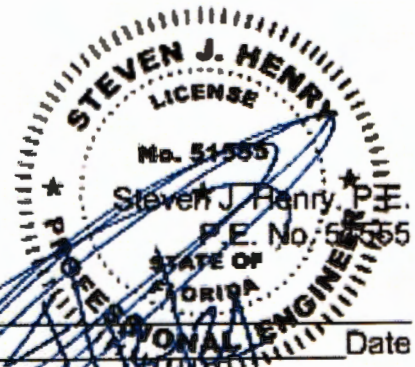
813-289-0039

State of Florida Authorization No. EB0004638

Revised April, 2024

November, 2023

Project No. 22050



INTRODUCTION

The purpose of this report is to provide an Access Management Analysis for the proposed development located south of Glen Lakes Boulevard and west of US 19 in Hernando County, Florida, as shown in Figure 1. The subject property is proposed to be developed for the following land uses:

- Assisted Living – 210 Beds
- Hotel – 80 Rooms
- Mini-Warehouse – 100,000 Square Feet
- Fast Food Restaurant w/Drive Thru – 3,000 Square Feet
- Fast Food Restaurant w/Drive Thru – 4,500 Square Feet
- Convenience Store/Gas Station – 5,200 Square Feet / 12 Fueling Positions
- Fast Food Restaurant w/Drive Thru – 3,500 Square Feet
- Medical-Dental Office Building – 20,000 Square Feet

The access for the project is proposed to be as follows:

- One (1) full access to Glen Lakes Boulevard to align with Outer Banks Road
- One (1) right-in/right-out access to US 19

This report will evaluate the access to serve the project. A copy of the site plan is included in the Appendix of this report.

ESTIMATED DAILY PROJECT TRAFFIC

The trip rates utilized in this report were obtained from the latest computerized version of





TABLE 1
ESTIMATED DAILY PROJECT TRIP ENDS

<u>Land Use</u>	<u>ITE LUC</u>	<u>Size</u>	<u>Daily Trip Ends (1)</u>	<u>Passerby Capture (2)</u>	<u>New Daily Trip Ends</u>
Assisted Living	254	210 Beds	546	0	546
Hotel	310	80 Rooms	639	0	639
Mini-Warehouse	151	100,000 SF	145	0	145
Fast Food Restaurant w/Drive Thru	934	3,000 SF	1,402	382	1,020
Fast Food Restaurant w/Drive Thru	934	4,500 SF	2,104	573	1,531
Convenience Store/Gas Station	945	5,200 SF	3,642	1,487	2,155
Fast Food Restaurant w/Drive Thru	934	3,500 SF	1,636	445	1,191
Medical-Dental Office Building	720	20,000 SF	<u>720</u>	<u>0</u>	<u>720</u>
		Total	10,834	2,887	7,947

(1) Source - ITE Trip Generation Manual, 11th Edition, 2021.

(2) Source - ITE Trip Generation Manual, 11th Edition, 2021.

• Passerby Percentage:

Fast Food Restaurant w/Drive Thru - 50%

(3,000 SF) 1,402 x 0.50 = 701

(4,500 SF) 2,104 x 0.50 = 1,052

(3,500 SF) 1,636 x 0.50 = 818

Convenience Store/Gas Station - 75%

3,642 x 0.75 = 2,732

• Passerby should not exceed 10% of the adjacent street traffic.

28,867(a) x 0.10 = 2,887 < 5,303

(a) Based on PM peak hour background traffic at the intersection of US 19 and Glen Lakes Boulevard and K = 0.09.

• Adjust passerby trips to 2,887 trip ends.



TABLE 2

ESTIMATED AM PEAK HOUR PROJECT TRIP ENDS

Land Use	ITE LUC	Size	AM Peak Hour Trip Ends (1)			Passerby Capture (2)			New AM Peak Hour Trip Ends		
			In	Out	Total	In	Out	Total	In	Out	Total
Assisted Living	254	210 Beds	23	15	38	0	0	0	23	15	38
Hotel	310	80 Rooms	18	15	33	0	0	0	18	15	33
Mini-Warehouse	151	100,000 SF	5	4	9	0	0	0	5	4	9
Fast Food Restaurant w/Drive Thru	934	3,000 SF	68	66	134	14	14	28	54	52	106
Fast Food Restaurant w/Drive Thru	934	4,500 SF	103	98	201	22	20	42	81	78	159
Convenience Store/Gas Station	945	12 FP	162	162	324	50	50	100	112	112	224
Fast Food Restaurant w/Drive Thru	934	3,500 SF	80	76	156	17	16	33	63	60	123
Medical-Dental Office Building	720	20,000 SF	<u>45</u>	<u>12</u>	<u>57</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>45</u>	<u>12</u>	<u>57</u>
		Total	504	448	952	103	100	203	401	348	749

(1) Source - ITE Trip Generation Manual, 11th Edition, 2021.

(2) Source - ITE Trip Generation Manual, 11th Edition, 2021.

• Passerby Percentage:

Fast Food Restaurant w/Drive Thru - 50%

(3,000 SF)

In: 68 x 0.50 = 34

Out: 66 x 0.50 = 33

(4,500 SF)

In: 103 x 0.50 = 52

Out: 98 x 0.50 = 49

(3,500 SF)

In: 80 x 0.50 = 40

Out: 76 x 0.50 = 38

Convenience Store/Gas Station - 76%

In: 162 x 0.76 = 123

Out: 162 x 0.76 = 123

• Passerby should not exceed 10% of the AM peak hour Background traffic.

2,031(a) x 0.10 = 203 < 492

(a) AM peak hour background traffic at the intersection of US 19 and Glen Lakes Boulevard.

• Adjust passerby trips to 203.



TABLE 3

ESTIMATED PM PEAK HOUR PROJECT TRIP ENDS

Land Use	ITE LUC	Size	PM Peak Hour Trip Ends (1)			Passerby Capture (2)			New PM Peak Hour Trip Ends		
			In	Out	Total	In	Out	Total	In	Out	Total
Assisted Living	254	210 Beds	20	30	50	0	0	0	20	30	50
Hotel	310	80 Rooms	16	15	31	0	0	0	16	15	31
Mini-Warehouse	151	100,000 SF	7	8	15	0	0	0	7	8	15
Fast Food Restaurant w/Drive Thru	934	3,000 SF	51	48	99	18	16	34	33	32	65
Fast Food Restaurant w/Drive Thru	934	4,500 SF	77	72	149	26	25	51	51	47	98
Convenience Store/Gas Station	945	5,200 SF	142	142	284	67	67	134	75	75	150
Fast Food Restaurant w/Drive Thru	934	3,500 SF	60	56	116	21	20	41	39	36	75
Medical-Dental Office Building	720	20,000 SF	<u>23</u>	<u>55</u>	<u>78</u>	<u>0</u>	<u>0</u>	<u>0</u>	<u>23</u>	<u>55</u>	<u>78</u>
		Total	396	426	822	132	128	260	264	298	562

(1) Source - ITE Trip Generation Manual, 11th Edition, 2021.

(2) Source - ITE Trip Generation Manual, 11th Edition, 2021.

• Passerby Percentage:

Fast Food Restaurant w/Drive Thru - 55%

(3,000 SF)

In: 51 x 0.55 = 28

Out: 48 x 0.55 = 26

(4,500 SF)

In: 77 x 0.55 = 42

Out: 72 x 0.55 = 40

(3,500 SF)

In: 60 x 0.55 = 33

Out: 56 x 0.55 = 31

Convenience Store/Gas Station - 75%

In: 142 x 0.75 = 106

Out: 142 x 0.75 = 106

• Passerby should not exceed 10% of the PM peak hour Background traffic.

2,598(a) x 0.10 = 260 < 412

(a) PM peak hour background traffic at the intersection of US 19 and Glen Lakes Boulevard.

• Adjust passerby trips to 260.

LEGEND

33 = NEW PROJECT TRAFFIC
 <36> = PASSERBY PROJECT TRAFFIC



04/11/24

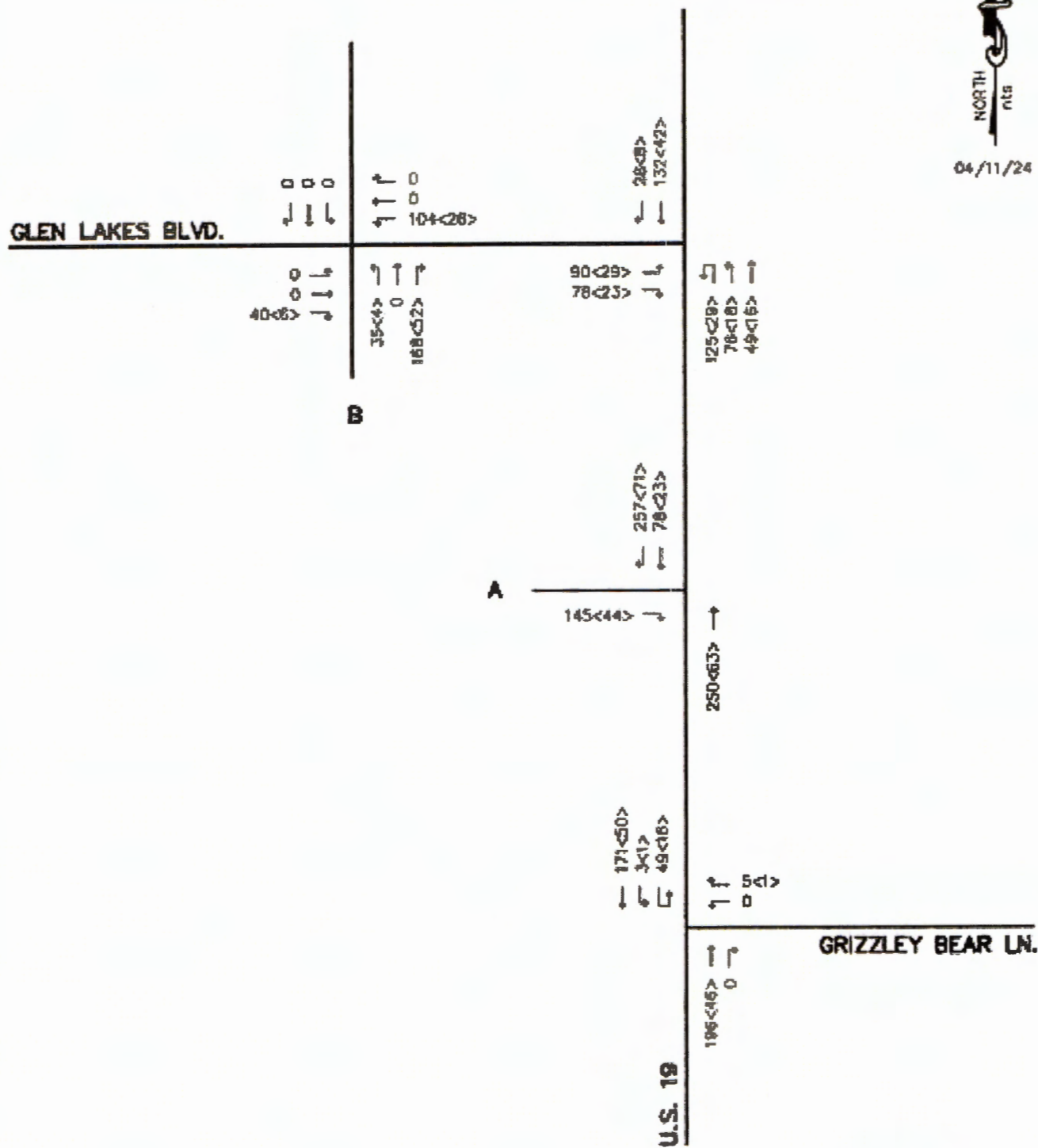
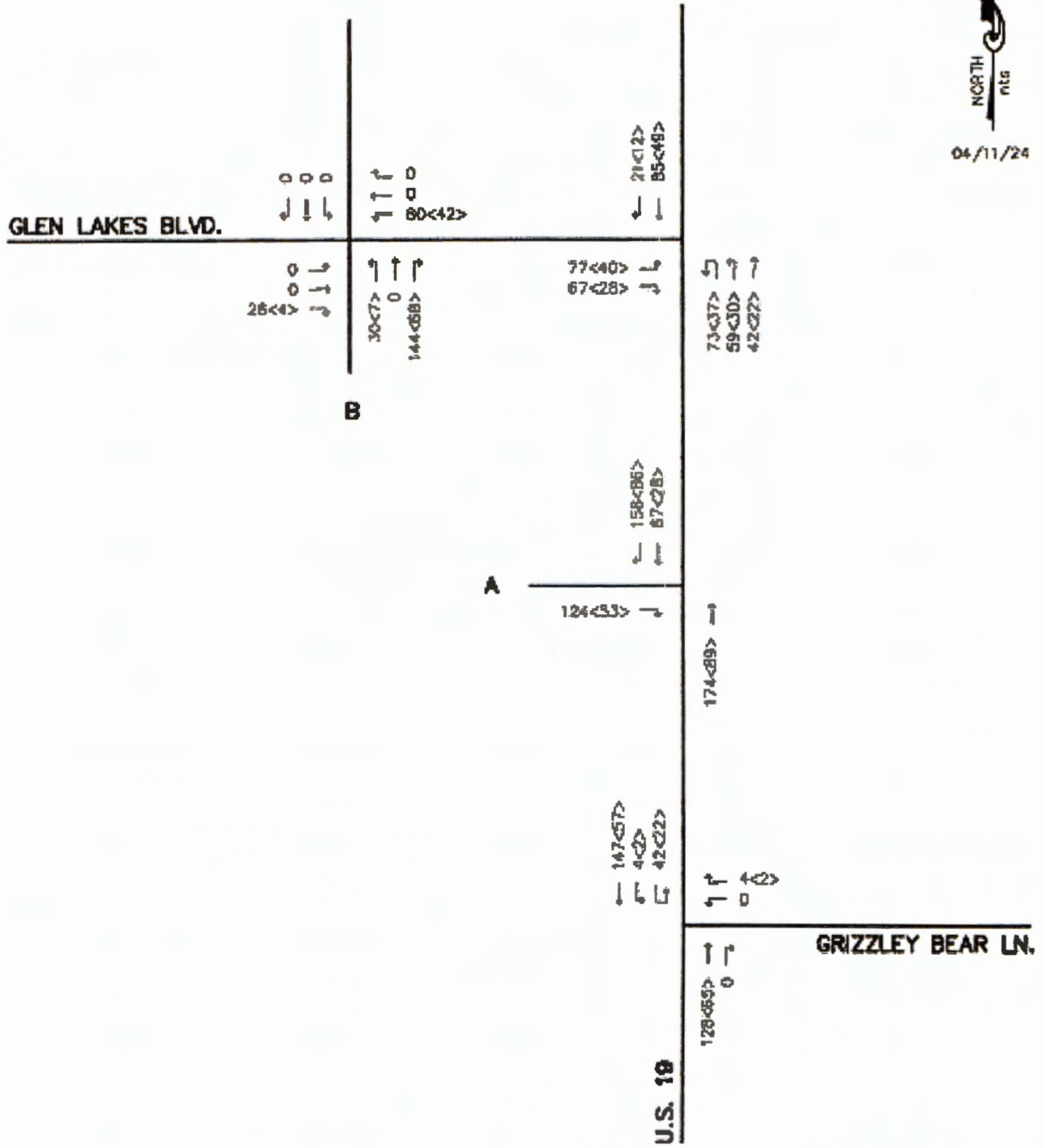


FIGURE 2
AM PEAK HOUR
PROJECT TRAFFIC



LEGEND

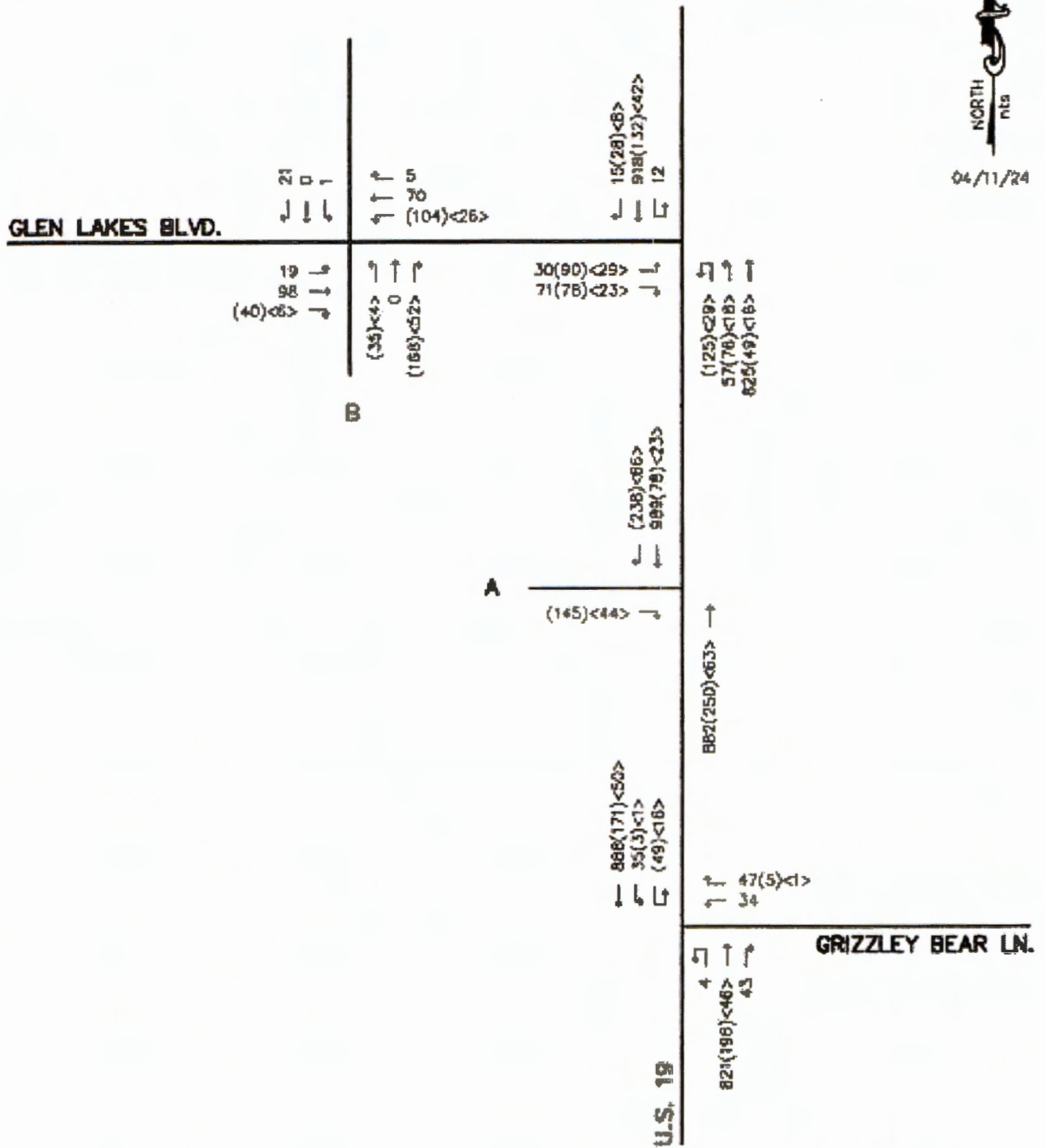
SS = NEW PROJECT TRAFFIC
 <36> = PASSERBY PROJECT TRAFFIC



**FIGURE 3
 PM PEAK HOUR
 PROJECT TRAFFIC**

LEGEND

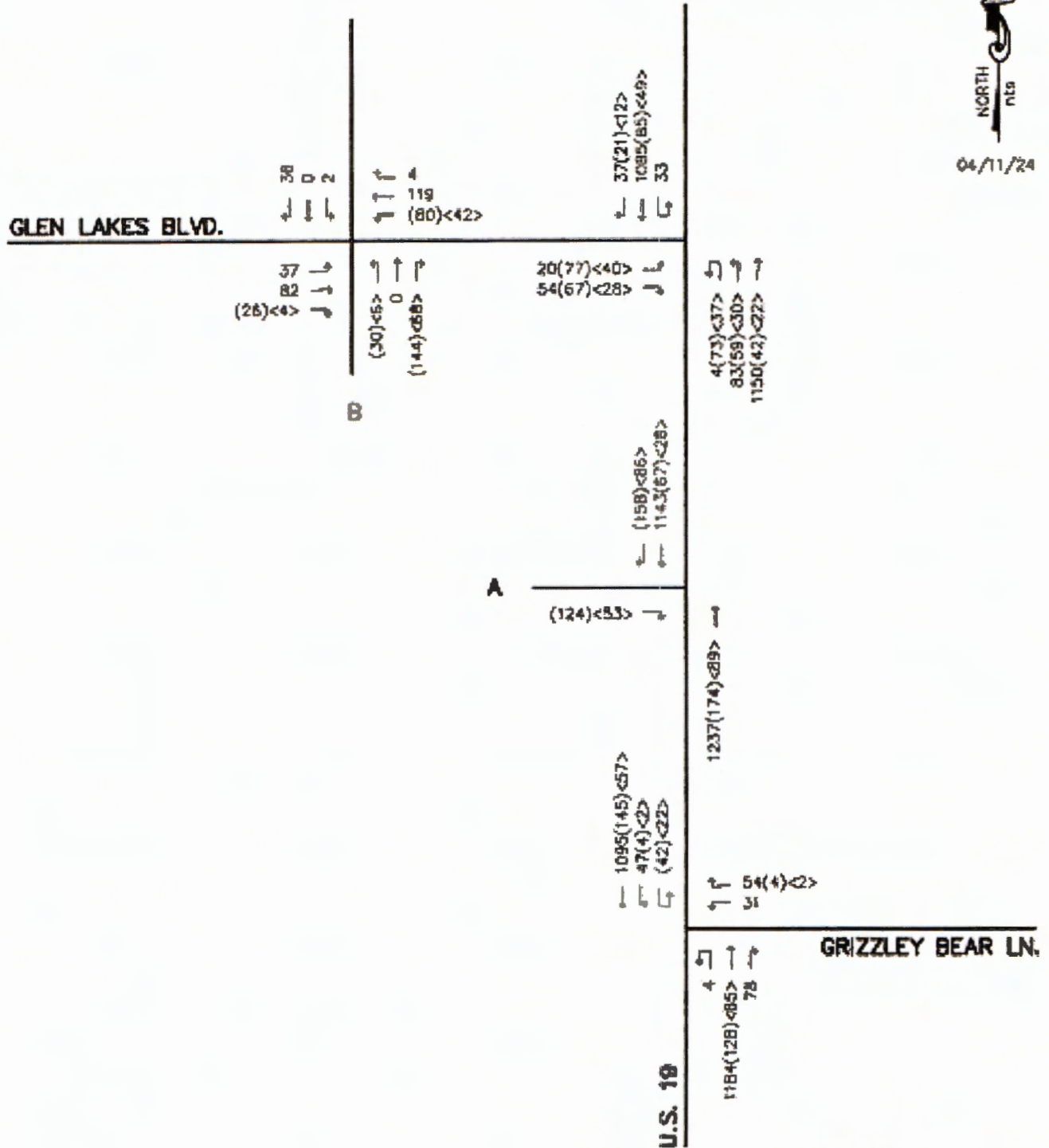
- 15 = EXISTING TRAFFIC
- (23) = NEW PROJECT TRAFFIC
- <36> = PASSERBY PROJECT TRAFFIC



**FIGURE 7
AM PEAK HOUR
BACKGROUND PLUS
PROJECT TRAFFIC**

LEGEND

- 13 = EXISTING TRAFFIC
- (23) = NEW PROJECT TRAFFIC
- <38> = PASSERBY PROJECT TRAFFIC



**FIGURE 8
PM PEAK HOUR
BACKGROUND PLUS
PROJECT TRAFFIC**

Timings

1: US 19 & Glen Lakes Blvd

04/11/2024



Lane Group	EBL	EBR	NBL	NBT	SBU	SBT	SBR
Lane Configurations							
Traffic Volume (vph)	149	172	305	890	12	1092	51
Future Volume (vph)	149	172	305	890	12	1092	51
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles (%)	2%	8%	4%	6%	2%	5%	8%
Shared Lane Traffic (%)							
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	pm+ov
Protected Phases	4	5	5	2		6	4
Permitted Phases		4	2		6		6
Detector Phase	4	5	5	2	6	6	4
Switch Phase							
Minimum Initial (s)	5.0	5.0	5.0	7.0	7.0	7.0	5.0
Minimum Split (s)	12.5	10.5	10.5	14.5	14.5	14.5	12.5
Total Split (s)	30.0	40.0	40.0	110.0	70.0	70.0	30.0
Total Split (%)	21.4%	28.6%	28.6%	78.6%	50.0%	50.0%	21.4%
Yellow Time (s)	5.5	3.5	3.5	5.5	5.5	5.5	5.5
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Lead/Lag		Lead	Lead		Lag	Lag	
Lead-Lag Optimize?		Yes	Yes		Yes	Yes	
Recall Mode	None	C-Max	C-Max	C-Max	None	None	None
Act Effct Green (s)	18.1	64.5	108.9	106.9	62.5	62.5	88.1
Actuated g/C Ratio	0.13	0.46	0.78	0.76	0.45	0.45	0.63
v/c Ratio	0.74	0.28	0.62	0.39	0.06	0.81	0.06
Control Delay	77.6	21.4	32.6	6.4	23.1	38.7	2.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	77.6	21.4	32.6	6.4	23.1	38.7	2.2
LOS	E	C	C	A	C	D	A
Approach Delay	47.5			13.1		37.0	
Approach LOS	D			B		D	

Intersection Summary

Cycle Length: 140

Actuated Cycle Length: 140

Offset: 0 (0%), Referenced to phase 2:NBT and 5:NBL, Start of Green

Natural Cycle: 80

Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.81

Intersection Signal Delay: 27.5

Intersection LOS: C

Intersection Capacity Utilization 72.4%

ICU Level of Service C

Analysis Period (min) 15

Splits and Phases: 1: US 19 & Glen Lakes Blvd



HCM Signalized Intersection Capacity Analysis

1: US 19 & Glen Lakes Blvd

04/11/2024



Movement	EBL	EBR	NBL	NBT	SBU	SBT	SBR
Lane Configurations	↙	↗	↙	↑↑	↓	↑↑	↗
Traffic Volume (vph)	149	172	305	890	12	1092	51
Future Volume (vph)	149	172	305	890	12	1092	51
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Lane Util. Factor	1.00	1.00	1.00	0.95	1.00	0.95	1.00
Frt	1.00	0.85	1.00	1.00	1.00	1.00	0.85
Flt Protected	0.95	1.00	0.95	1.00	0.95	1.00	1.00
Satd. Flow (prot)	1770	1495	1736	3406	1770	3438	1495
Flt Permitted	0.95	1.00	0.09	1.00	0.28	1.00	1.00
Satd. Flow (perm)	1770	1495	157	3406	528	3438	1495
Peak-hour factor, PHF	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Adj. Flow (vph)	169	195	347	1011	14	1241	58
RTOR Reduction (vph)	0	15	0	0	0	0	25
Lane Group Flow (vph)	169	180	347	1011	14	1241	33
Heavy Vehicles (%)	2%	8%	4%	6%	2%	5%	8%
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	pm+ov
Protected Phases	4	5	5	2		6	4
Permitted Phases		4	2		6		6
Actuated Green, G (s)	18.1	57.0	106.9	106.9	62.5	62.5	80.6
Effective Green, g (s)	18.1	57.0	106.9	106.9	62.5	62.5	80.6
Actuated g/C Ratio	0.13	0.41	0.76	0.76	0.45	0.45	0.58
Clearance Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	228	608	558	2600	235	1534	940
v/s Ratio Prot	c0.10	0.08	c0.17	0.30		c0.36	0.00
v/s Ratio Perm		0.04	0.30		0.03		0.02
v/c Ratio	0.74	0.30	0.62	0.39	0.06	0.81	0.04
Uniform Delay, d1	58.7	28.0	30.8	5.6	22.0	33.6	12.9
Progression Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Incremental Delay, d2	12.2	0.3	5.1	0.4	0.1	3.3	0.0
Delay (s)	70.9	28.3	36.0	6.0	22.1	36.8	12.9
Level of Service	E	C	D	A	C	D	B
Approach Delay (s)	48.1			13.7		35.6	
Approach LOS	D			B		D	

Intersection Summary			
HCM 2000 Control Delay	27.3	HCM 2000 Level of Service	C
HCM 2000 Volume to Capacity ratio	0.74		
Actuated Cycle Length (s)	140.0	Sum of lost time (s)	20.5
Intersection Capacity Utilization	72.4%	ICU Level of Service	C
Analysis Period (min)	15		
c Critical Lane Group			

Timings

1: US 19 & Glen Lakes Blvd

04/11/2024



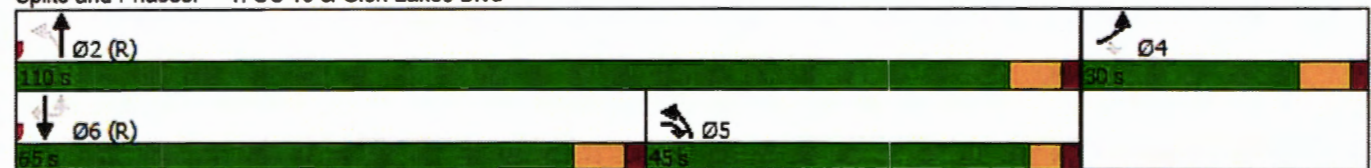
Lane Group	EBL	EBR	NBL	NBT	SBU	SBT	SBR
Lane Configurations							
Traffic Volume (vph)	137	149	286	1214	33	1219	70
Future Volume (vph)	137	149	286	1214	33	1219	70
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Heavy Vehicles (%)	2%	9%	2%	3%	2%	5%	6%
Shared Lane Traffic (%)							
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	Perm
Protected Phases	4	5	5	2		6	
Permitted Phases		4	2		6		6
Detector Phase	4	5	5	2	6	6	6
Switch Phase							
Minimum Initial (s)	5.0	5.0	5.0	7.0	7.0	7.0	7.0
Minimum Split (s)	12.5	10.5	10.5	14.5	14.5	14.5	14.5
Total Split (s)	30.0	45.0	45.0	110.0	65.0	65.0	65.0
Total Split (%)	21.4%	32.1%	32.1%	78.6%	46.4%	46.4%	46.4%
Yellow Time (s)	5.5	3.5	3.5	5.5	5.5	5.5	5.5
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Lead/Lag		Lag	Lag		Lead	Lead	Lead
Lead-Lag Optimize?		Yes	Yes		Yes	Yes	Yes
Recall Mode	None	None	None	C-Min	C-Min	C-Min	C-Min
Act Effct Green (s)	16.3	50.3	110.7	108.7	76.7	76.7	76.7
Actuated g/C Ratio	0.12	0.36	0.79	0.78	0.55	0.55	0.55
v/c Ratio	0.68	0.28	0.58	0.46	0.17	0.66	0.08
Control Delay	75.3	27.3	27.9	6.4	22.9	26.4	4.7
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	75.3	27.3	27.9	6.4	22.9	26.4	4.7
LOS	E	C	C	A	C	C	A
Approach Delay	50.3			10.5		25.2	
Approach LOS	D			B		C	

Intersection Summary

Cycle Length: 140
 Actuated Cycle Length: 140
 Offset: 0 (0%), Referenced to phase 2:NBTU and 6:SBTU, Start of Green
 Natural Cycle: 65
 Control Type: Actuated-Coordinated
 Maximum v/c Ratio: 0.68
 Intersection Signal Delay: 20.4
 Intersection Capacity Utilization 74.2%
 Analysis Period (min) 15

Intersection LOS: C
 ICU Level of Service D

Splits and Phases: 1: US 19 & Glen Lakes Blvd



HCM Signalized Intersection Capacity Analysis

1: US 19 & Glen Lakes Blvd

04/11/2024



Movement	EBL	EBR	NBL	NBT	SBU	SBT	SBR
Lane Configurations							
Traffic Volume (vph)	137	149	286	1214	33	1219	70
Future Volume (vph)	137	149	286	1214	33	1219	70
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Lane Util. Factor	1.00	1.00	1.00	0.95	1.00	0.95	1.00
Frt	1.00	0.85	1.00	1.00	1.00	1.00	0.85
Flt Protected	0.95	1.00	0.95	1.00	0.95	1.00	1.00
Satd. Flow (prot)	1770	1482	1770	3505	1770	3438	1524
Flt Permitted	0.95	1.00	0.15	1.00	0.20	1.00	1.00
Satd. Flow (perm)	1770	1482	276	3505	364	3438	1524
Peak-hour factor, PHF	0.98	0.98	0.98	0.98	0.98	0.98	0.98
Adj. Flow (vph)	140	152	292	1239	34	1244	71
RTOR Reduction (vph)	0	12	0	0	0	0	32
Lane Group Flow (vph)	140	140	292	1239	34	1244	39
Heavy Vehicles (%)	2%	9%	2%	3%	2%	5%	6%
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	Perm
Protected Phases	4	5	5	2		6	
Permitted Phases		4	2		6		6
Actuated Green, G (s)	16.3	42.8	110.7	108.7	76.7	76.7	76.7
Effective Green, g (s)	16.3	42.8	110.7	108.7	76.7	76.7	76.7
Actuated g/C Ratio	0.12	0.31	0.79	0.78	0.55	0.55	0.55
Clearance Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	206	511	501	2721	199	1883	834
v/s Ratio Prot	c0.08	0.05	c0.11	0.35		c0.36	
v/s Ratio Perm		0.04	0.35		0.09		0.03
v/c Ratio	0.68	0.27	0.58	0.46	0.17	0.66	0.05
Uniform Delay, d1	59.3	36.8	24.5	5.4	15.8	22.4	14.7
Progression Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Incremental Delay, d2	8.6	0.3	1.7	0.6	1.9	1.8	0.1
Delay (s)	68.0	37.1	26.2	6.0	17.6	24.3	14.8
Level of Service	E	D	C	A	B	C	B
Approach Delay (s)	51.9			9.8		23.6	
Approach LOS	D			A		C	

Intersection Summary			
HCM 2000 Control Delay	19.6	HCM 2000 Level of Service	B
HCM 2000 Volume to Capacity ratio	0.65		
Actuated Cycle Length (s)	140.0	Sum of lost time (s)	20.5
Intersection Capacity Utilization	74.2%	ICU Level of Service	D
Analysis Period (min)	15		
c Critical Lane Group			

COST ESTIMATE



ENGINEER'S CONSTRUCTION COST ESTIMATE

SIGNALIZATION PROJECT U.S. 19 @ GLEN LAKES BOULEVARD HERNANDO COUNTY, FLORIDA

Estimated By: Robert W. Fulp, PE (Engineer-of-Record)
Company: Lincks & Associates, Inc.
Date: 7-Feb-25
Project No.: 22050 / F4

Roadway	\$	171,012	(1)
Signing / Pavement Markings	\$	15,000	(2)
Signalization & Lighting	\$	796,025	(1)
Sub-Total	\$	982,037	
Contingency Items (10%)	\$	98,204	(3)
Maintenance of Traffic (5%)	\$	49,102	
Mobilization (5%)	\$	49,102	
Bonding (3%)	\$	29,461	
MOT Plans	\$	3,500	(2)
Construction Surveying	\$	5,000	(2)
Permits	\$	2,000	(2)
Material Testing	\$	5,000	(2)
As-Builts	\$	3,500	(2)
Sub-Total	\$	1,226,906	
Independent Contractors Fee (8%)	\$	98,152	
Grand Total	\$	1,325,058	

Notes:

(1) The estimate was based on FDOT's current 12-month (12/1/23 - 11/31/24) moving average unit costs for Area 7, which includes Hernando County. See attached plans.

(2) This is an estimated value based on the size of the project.

(3) Due to project unknowns, such as unexpected utility relocations, etc., a contingency of 10% was assumed.

PROPORTIONATE SHARE CALCULATION





TABLE A-1
PROPORTIONATE SHARE CALCULATION

<u>Intersection</u>	<u>Time Period</u>	<u>Critical Movement</u>	<u>LOS E Lane Group Capacity</u>	<u>LOS D Lane Group Capacity (1)</u>	<u>New Project Trips</u>
US 19 and Glen Lakes Blvd	AM	EB Left	228	202	119
		NB Left	558	494	248
		SB Through	1,534	<u>1,358</u>	<u>174</u>
				2,054	541
				% Consumed	26.34%
	PM	EB Left	206	182	117
NB Left		501	443	199	
SB Through		1,883	<u>1,666</u>	<u>134</u>	
				2,291	450
				% Consumed	19.64%

(1) Adjusted by 0.885