GLEN LAKE'S COMMONS SIGNALIZED INTERSECTION CONSTRUCTION AGREEMENT

THIS AGREEMENT (this "Agreement") is made and entered into this 3th day of point and county, a political subdivision of the State of Florida ("County") and Glen Lake's Commons, LLC ("GLC"). The County and GLC shall each be referred to herein as a "Party" and shall be collectively referred to herein as the "Parties."

RECITALS

WHEREAS GLC is the developer of the approximate 21.38-acre property described as Glen Lake's Commons commercial development ("GLC Project", as further described in Exhibit "A"); and

WHEREAS, the County has planned a to-be-named county frontage road connecting from the south to the north Glen Lakes Boulevard and Bourassa Boulevard along the west side of U.S. Highway 19, as approved by the Hernando County engineer; and

WHEREAS, GLC desires to cooperatively to assist the County in constructing the Signalized Intersection Project (as defined below) as specifically provided for in this Agreement; and

WHEREAS, the Parties desire to enter into this Agreement in order to provide for the rights and responsibilities of the Parties regarding the design, permitting, construction and funding of the Signalized Intersection Project.

NOW, THEREFORE, in consideration of the covenants hereinafter contained, and other good and valuable consideration, the receipt of which is hereby acknowledged, the Parties agree as follows:

AGREEMENT

- 1. <u>Recitals</u>. The above referenced recitals are true and correct and are incorporated herein by reference.
- Signalized Intersection Relocation Project Plans and Specifications. The Parties plan for a signalized intersection as depicted in Exhibit "B". The Parties agree to the plans and design specifications for the Signalized Intersection Project which are contained in the "Construction Plans for the U.S. Highway 19 and Glen Lakes Blvd Intersection Improvements" prepared by Lincks & Associates, Inc. ("Lincks") and attached hereto as Composite Exhibit "C" ("Plans and Specifications"). Any material modifications to the Plans and Specifications shall require the approval of the Parties, which approval may be confirmed in a separate written instrument, including electronic format, by each Party. The County agrees that any approval for such modifications may be administratively approved and delivered by the Director of Public Works/County Engineer.
- 3. The County's Role Project Manager. The Parties agree that the County shall be the project manager responsible for the design, permitting, construction, and funding of the Signalized Intersection Relocation Project as provided in this Agreement. Within ten (10) business days of the Effective Date of this Agreement, the County will coordinate with the Florida Department of Transportation ("FDOT") to assume the role of applicant and permittee with respect to the required FDOT application for construction, and take all necessary actions to obtain FDOT's approval of the Plans and Specifications, together with any other required permits to effectuate the construction

of the Signalized Intersection Project including the finalization and execution by the County of the FDOT Construction Agreement (collectively, "Government Authorizations"). The County shall use reasonable efforts to obtain all required Governmental Authorizations for the Signalized Intersection Project within ninety (90) calendar days of the Effective Date of this Agreement. Any modifications to the Plans and Specifications must be approved by the Parties as set forth in Paragraph 2 above.

- Project General Contractor, Engineer, and Bidding Process for Final Section of Construction 4. Contractor. The Parties agree that GLC shall be the independent contractor responsible for the management of the construction of the Signalized Intersection Project, and Lincks shall continue to serve as the engineer. GLC agrees to solicit sealed alternate competitive bids to determine the Final Project Costs (defined below) for the Signalized Intersection Project and select the construction contractor. The final selection of the construction contractor shall be based upon the lowest reasonable and responsive alternate competitive bid. GLC shall reasonably attempt to obtain at least three (3) competitive bids, if possibly. GLC agrees to otherwise substantially comply with the County's bid process. Each bid shall clearly indicate the cost of each component of the Signalized Intersection Project. GLC also agrees County personnel may, but are not obligated to, be present when the sealed bids are opened. GLC further agrees to allow the County to concurrently approve and accept the most reasonable and responsive low bid, and to approve contract documents and related payment terms prepared by GLC. The bid award for the Signalized Intersection Project will require Hernando County Commission approval. The County shall be responsible for entering into all agreements with the construction contractor, Lincks and other professionals/consultants performing work on the Signalized Intersection Project and, subject to Paragraph 6 below, shall be the responsible party for paying all contractors, professionals and consultants directly.
- 5. Estimated and Final Project Costs. Attached as Exhibit "D" is an estimate in the amount of \$1,325,058.00 from Lincks which represents the design, permitting and construction costs for the Signalized Intersection Project ("Estimated Project Costs"). The final Signalized Intersection Project costs shall be the total costs for the Signalized Intersection Project, including without limitation design, permitting and construction costs, upon the final completion of the Signalized Intersection Project on the date that the County obtains approval of the final Signalized Intersection Project as-built from all applicable governmental permitting entities, including FDOT and the County ("Final Project Costs").
- 6. Cost-Sharing. Attached as Exhibit "E" is the transportation analysis prepared by Lincks dated January 29, 2025 ("Transportation Analysis") which provides a proportionate share allocation for the Signalized Intersection Project. The Transportation Analysis assigns Seventy-Three and Sixty-Six Hundredths Percent (73.66%) of the Final Project Costs to the County which equates to \$976,037.72, based on the Estimated Project Costs, and Twenty-Six and Thirty-Four Hundredths Percent (26.34%) of the Final Project Costs to GLC, which equates to \$349,020.28, based on the Estimated Project Costs (the "GLC Estimated Contribution"). The Parties agree that the Final Project Costs shall be paid as follows:
 - a. Following the Effective Date and during the construction of the Signalized Intersection Project, GLC shall submit paid invoices for any and all out-of-pocket expenses related to the Signalized Intersection Project, including without limitation fees and costs paid to Lincks and other approved "soft costs" set forth in Exhibit "D" (the "GLC Expenses"). The GLC Expenses shall be credited toward the GLC Estimated Contribution.

- b. Subject to the County's determination in accordance with Hernando County Code of Ordinances Section 23-144 that the Signalized Intersection Project is impact fee creditable, the Parties agree that any and all impact fees payable to the County by purchasers or tenants of parcels within the GLC Project which become due prior to completion of the Signalized Intersection Project (collectively, "GLC Credits") shall be credited toward the GLC Estimated Contribution.
- c. Upon GLC's payment of GLC Expenses and/or application of GLC Credits in a cumulative sum equal to the GLC Estimated Contribution, GLC shall have no further monetary obligations with respect to the Signalized Intersection Project. The County shall be solely responsible for paying Final Project Costs that exceed the GLC Estimated Contribution.
- d. No later than one hundred twenty (120) calendar days after completion of the construction of the Signalized Intersection Project, the County shall provide GLC with itemized documentation of the Final Construction Costs paid by the County and with respect to the entirety of the Signalized Intersection Project.
- e. In the event that the Final Project Costs are less than the Estimated Project Costs, the County shall refund GLC's overpaid portion of the GLC Estimated Contribution no later than one hundred eighty (180) calendar days after completion of the Signalized Intersection Project. The Parties hereby agree that such refund shall be in the form of transportation impact fee credits, if available, or, if not available, in the form of immediately available funds.
- 7. Pre-Construction Conference. GLC shall hold a pre-construction conference for the Signalized Intersection Project and notify the County so the County may attend and participate, at which time GLC shall provide the County with a proposed notice to contractor(s) to commence ordering materials and prepare shop drawings within thirty (30) days.
- 8. <u>Inspection</u>. GLC agrees to permit the County inspector(s) to be always present during construction of the Signalized Intersection Project, subject to any and all applicable workplace safety regulations.
- 9. <u>Remedies.</u> In the event of a breach of any Party's obligations under this Agreement, a cause of action shall immediately accrue to the non-breaching Party, and such non-breaching Party shall be entitled to pursue all remedies in law and equity.
- 10. Notices. All notices which are required or permitted hereunder must be in writing and shall be deemed to have been given, delivered or made, as the case may be (notwithstanding lack of actual receipt by the addressee) (i) upon hand delivery; (ii) one (1) business day after having been deposited with an expedited, overnight courier service (such as by way of example but not limitation, U.S. Express Mail, Federal Express, or UPS), or (iii) upon delivery of a facsimile transmission with electronic delivery verification to the following addresses or numbers:

If to GLC:

Glen Lake's Commons, LLC 2400 Tamiami Trail North

Suite 400

Naples, Florida 34103

E-mail: bob@sitetokey.com; clay@wbdcinc.com

Courtesy copy to: Spencer Fane LLP

201 North Franklin Street

Suite 2150

Tampa, Florida 33602 Attn: Derek Larsen-Chaney Fax: (813) 405-8904

E-mail: dlarsenchaney@spencerfane.com

If to the County: Hernando County

15470 Flight Path Drive Brooksville, FL 34604 Attn: County Administrator

Fax (352) 754-4477

Email: administration@co.hernando.fl.us

Courtesy copy to: Hernando County Attorney's Office

20 N. Main St., Ste. 462 Brooksville, FL 34601 Attn: Garth Coller, Esq. Fax: (352) 754-4001

E-mail: CAO@hernandocounty.us

The failure by any party to deliver a courtesy copy as referenced above shall not constitute a default under the terms of this Agreement nor shall it create a defect in any notice which is otherwise properly given. Furthermore, it is agreed that, if any party hereto is represented by legal counsel, such legal counsel is authorized to deliver written notice directly to the other party on behalf of his or her client, and the same shall be deemed proper notice hereunder if delivered in the manner hereinabove specified. Any party hereto may, at any time by giving ten (10) business days written notice to the other party hereto, designate any other address in substitution of the foregoing address to which such notice shall be given and other parties to whom copies of all notices hereunder shall be sent.

11. Miscellaneous.

- a. <u>Participation</u>. Each of the Parties have participated fully in the negotiation and preparation of this Agreement with full benefit of counsel. Accordingly, this Agreement shall not be more strictly construed against any of the Parties, and shall be interpreted as if the Parties hereto jointly prepared it.
- b. <u>Cooperation</u>. The Parties shall execute in good faith such other and further documents as may be required to effectuate the terms of this Agreement.
- c. <u>Force Majeure</u>. If the performance of any obligation under this Agreement is prevented, restricted, or interfered with by reason of war, revolution, civil commotion, acts of public enemies, pandemic, epidemic, blockade, embargo, strikes, acts of God, floods, tropical storms or hurricanes, then the affected party shall, upon giving prior written notice to the other party, be excused from such performance to the extent of such prevention, restriction, or interference, provided that the affected party shall use commercially reasonable efforts to avoid or remove such causes of nonperformance, and shall continue performance hereunder with reasonable dispatch whenever such causes are removed.

- d. Attorneys' Fees/Venue. In the event of any dispute, litigation, or other proceeding between the Parties arising out of this Agreement, to enforce any provision of this Agreement, or any right of the Parties hereunder, each party to such dispute, litigation, or other proceeding shall pay its own attorney fees, costs and expenses incurred in court, at trial, on appeal, and in any other proceeding irrespective of whether a party prevails in such litigation or proceeding. In the event of litigation, venue shall be in Hernando County, Florida. The provisions of this Section shall survive termination of this Agreement.
- e. Relationship of the Parties; No Third-Party Beneficiaries. Nothing contained in this Agreement is intended to, or shall, or shall be deemed to, create a joint venture or partnership of any kind between the Parties hereto. Nothing contained in this Agreement is intended to create any rights in third parties.
- f. Waiver of Jury Trial. THE PARTIES JOINTLY AND SEVERALLY, HEREBY KNOWINGLY, VOLUNTARILY, AND INTENTIONALLY WAIVE THE RIGHT THEY MAY HAVE TO A TRIAL BY JURY IN RESPECT OF ANY LITIGATION BASED HEREON, OR ARISING OUT OF, UNDER OR IN CONNECTION WITH THIS AGREEMENT AND ANY AGREEMENT CONTEMPLATED TO BE EXECUTED IN CONJUNCTION HEREWITH, OR ANY COURSE OF CONDUCT, COURSE OF DEALING, STATEMENTS, WHETHER VERBAL OR WRITTEN, OR ACTIONS OF EITHER PARTY.
- g. <u>Severability</u>. In the event any one or more provisions contained in this Agreement shall for any reason be held invalid, illegal, or unenforceable in any respect, such invalidity, illegality, or unenforceability shall not affect any other provision hereof and this Agreement shall be construed as if such invalid, illegal, or unenforceable provision had not been contained herein.
- h. <u>Assignment</u>. Assignment of this Agreement by any Party must be approved in writing by the other Parties, which approval shall not be unreasonably withheld.
- i. <u>Binding Effect</u>. This Agreement shall be binding upon and shall inure to the benefit of the successors and assigns of the respective Parties.
- j. <u>Authority</u>. If applicable, the entity officer or manager executing this Agreement certifies by acknowledgment of the signature below that s/he has been properly authorized to enter into this Agreement on behalf of, and binding with respect to, such entity.
- k. <u>Counterparts</u>. This Agreement may be executed in any number of counterparts, each of which when so executed and delivered shall be an original, but such counterparts shall together constitute but one and the same instrument.
- 1. <u>Amendment</u>. This Agreement is the complete agreement of the Parties and may not be changed, orally, but only by instrument in writing signed by the Parties.
- m. <u>Headings</u>. Titles and captions to paragraphs and sections are inserted for convenience only, and in no way define, limit, extend or describe the scope or intent of this Agreement or the paragraphs, sections or provisions herein.

- n. No Waiver. Failure of any Party to exercise any right or power given hereunder, or to insist upon compliance by another Party with its obligations set forth herein, shall not constitute a waiver of any Party's right to demand strict compliance with the terms and provisions of this Agreement. Parties further agree that the waiver of a Party's breach or threatened breach of any obligations under this Agreement shall not be construed as a waiver of any subsequent breach by that Party.
- o. <u>Business Day.</u> Business day means any day other than Saturday or Sunday or a legal holiday.
- p. <u>Effective Date</u>. The "Effective Date" of this Agreement shall be the date that the last of the Parties execute this Agreement.

(Remainder of page intentionally left blank. Signature page(s) to follow.)

IN WITNESS WHEREOF, the Parties hereto have set their hands and seals as of the dates set forth below.

HERNANDO COUNTY

	ATTEST:	
a	Douglas Chorvat Jr. Clerk of the Circuit County SEAL	Brian Hawkins Hernando County, Board Chairman Date: 4-8-2025
	STATE OF FLORIDA	
	COUNTY OF HERNANDO	
	The foregoing instrument was act online notarization, this 4th day of 1 Hernando County Board of County Commas identification	knowledged before me by means of physical presence or , 2025, by Brian Hawkins as Chairman of the nissioners, who is personally known to me or who has produced ation.
	[Notary Seal]	Notary Public Conko Name printed
	COLLEEN CONKO Notary Public - State of Florida Commission # HH 281269 My Comm. Expires Jun 27, 2026 Bonded through National Notary Assn.	My Commission Expires: © 6-27-2026

FOR THE USE AND RELIANCE OF HERNANDO COUNTY ONLY. APPROVED AS TO FORM AND LEGAL SUFFICIENCY.

Victoria Anderson

County Attorney's Office

WITNESSES:	GLEN LAKE'S COMMONS, LLC
Print Name: Ancana D. Cantrell Address: 11173 Riley Pincs Circle Chibsonton, R. 33534 Print Name: Elizabeth A. Contre Address: 7203 Meeting House Lare Apollo Beach, FL 33572	By: Name: Robert Conez Un. Title: Margar of Part 198 Date: 3/26/2025
STATE OF Florida COUNTY OF Hills borough	
The foregoing instrument was acknowledged online notarization, this 110th day of March Maraging Partner of Glen Lake's Commons, LLC, a known to me or who has produced	
[Nota LORI N. HARRIS MY COMMISSION # HH 368084 EXPIRES: June 14, 2027	Notary Public Lori N. Harris Name printed

My Commission Expires: June 14, 7025

Exhibit "A"

LEGAL DESCRIPTION FOR GLEN LAKE'S COMMONS

THAT PART OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, TOWNSHIP 22 SOUTH, RANGE 17 EAST, HERNANDO COUNTY, FLORIDA, LYING WEST OF U.S. HIGHWAY #19.

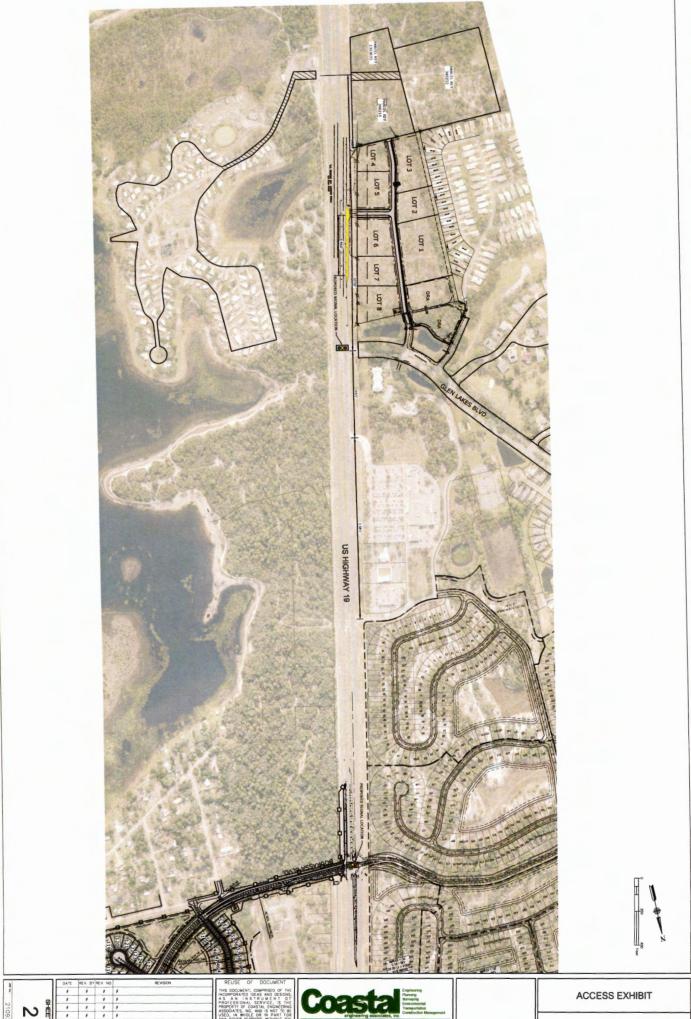
BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHWEST CORNER OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, TOWNSHIP 22 SOUTH, RANGE 17 EAST, HERNANDO COUNTY, FLORIDA; THENCE ALONG THE WEST LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, RUN N01°18'06"E, 1330.05 FEET TO THE NORTH LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SAID SECTION 13; THENCE N50°41'32"W, 57.35 FEET TO A NON-TANGENT CURVE TO THE RIGHT AND THE EAST RIGHT-OF-WAY LINE OF NEW ORLEANS DRIVE, SAID CURVE BEING CONCAVE TO THE SOUTHEAST, HAVING A RADIUS OF 175.00 FEET, A CENTRAL ANGLE OF 31°34'08", AND A CHORD BEARING AND DISTANCE OF N23°39'40"E, 95.21 FEET; THENCE ALONG THE ARC OF SAID CURVE 96.42; THENCE N39°20'26"E, 238.56 FEET; THENCE LEAVING SAID EAST RIGHT-OF-WAY LINE, RUN S50°40'56"E, 63.11 FEET; THENCE S47°48'06"E, 62.58 FEET; THENCE S24°09'32"E, 93.48 FEET: THENCE S53°13'38"E, 43.57 FEET; THENCE S80°46'20"E, 82.26 FEET TO A NON-TANGENT CURVE TO THE LEFT, AND THE WEST RIGHT-OF-WAY LINE OF OUTER BANKS DRIVE, SAID CURVE BEING CONCAVE TO THE EAST, HAVING A RADIUS OF 353.95 FEET. A CENTRAL ANGLE OF 7°52'58". AND A CHORD BEARING AND DISTANCE OF S05°18'47"W, 48.66 FEET: THENCE ALONG THE ARC OF SAID CURVE 48.70 FEET; THENCE S01°10'27"W, 62.01 FEET TO THE NORTH LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SAID SECTION 13; THENCE ALONG SAID NORTH LINE, RUN S88°40'06"E, 49.97 FEET TO THE EAST RIGHT-OF-WAY LINE OF OUTER BANKS DRIVE: THENCE CONTINUE ALONG SAID NORTH LINE, RUN S88°42'23"E, 350.66 FEET TO THE WEST RIGHT-OF-WAY LINE OF U.S. HIGHWAY #19; THENCE ALONG SAID WEST LINE, RUN S14°08'08"W. 1365.17 FEET: THENCE LEAVING SAID WEST LINE ALONG THE SOUTH LINE OF THE SOUTHEAST 1/4 OF THE SOUTHWEST 1/4 OF SECTION 13, RUN N88°33'43"W, 486.32 FEET TO THE POINT OF BEGINNING.

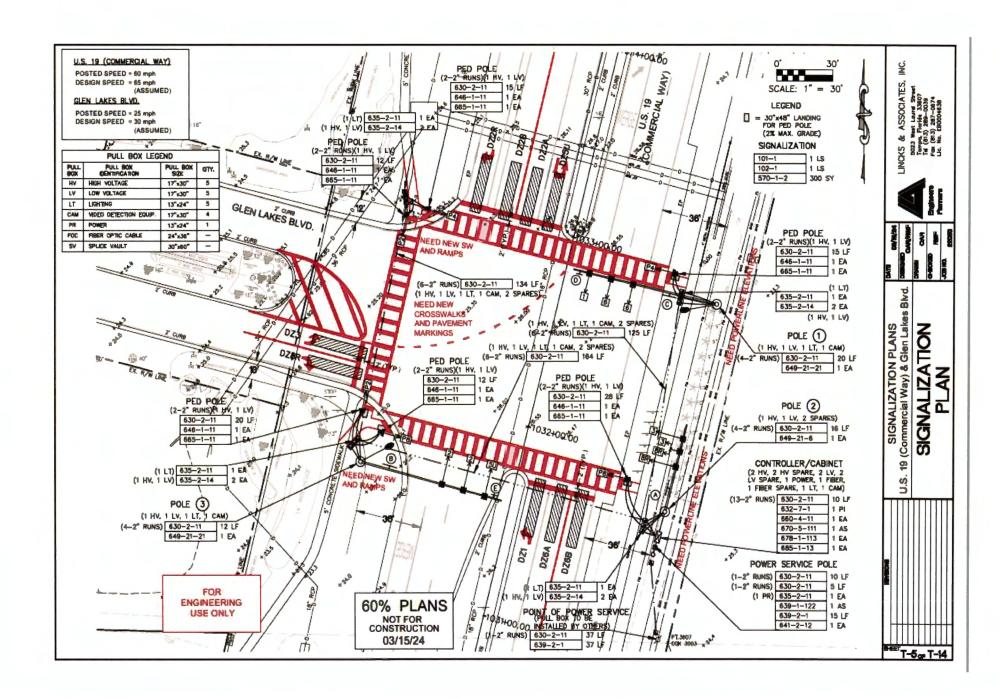
CONTAINING 21.38 ACRES MORE OR LESS.

Exhibit "B"

Depiction of Signalized Intersection



GLEN LAKES COMMONS



Composite Exhibit "C"

Plans and Specifications



ON THE DATE ADJACENT TO THE SEAL PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED AND THE SIGNATURE MUST BE VERIFIED ON ANY ELECTRONIC COPIES

SIGNALIZATION PLANS

FOR

U.S. 19 (Commercial Way) and Glen Lakes Blvd.

Hernando County, Florida

PREPARED FOR

Glen Lakes Common, LLC

13904 N. Boulevard Tampa, Florida 33613

GOVERNING STANDARD PLANS

FLORIDA DEPARTMENT OF TRANSPORTATION, FY 2024-25 STANDARD PLANS FOR ROAD AND BRIDGI CONSTRUCTION AND APPLICABLE INTERIM REVISIONS (IRs). STANDARD PLANS FOR ROAD CONSTRUCTION AND ASSOCIATED IRs ARE AVAILABLE AT THI FOLLOWING WEBSITE:

http://www.fdot.gov/design/standard.plan

APPLICALDE IRS: NONE
STANDARD PLANS FOR BRIDGE CONSTRUCTION ARE INCLUDED IN THE STRUCTURES PLAY
COMPONENT.

GOVERNING STANDARDS SPECIFICATIONS

FDOT CONSTRUCTION AGREEMENT #2024-C-798-XXXXX

60% PLANS

NOT FOR CONSTRUCTION

12/20/24

LINCKS & ASSOCIATES, LLC ENGINEERS PLANNERS

| T-1 | Cover Sheet | T-2 | Tabulation of Quantities | T-3/4 | General Notes | T-5/6 | Signalization Plan | T-7 | Lighting Plan | T-8 | Guide Sign Worksheet (Street Name Signs) | T-9 | Guide Sign Worksheet (Advance Street Signs) | T-10 | Mast Arm Tabulation | T-11 | Mast Arm Tabulation | T-11 | Mast Arm Assemblies | T-12 | Report of Core Borings | T-13 | Traffic Control General Notes | T-14 | Subsurface Utility Engineering Plan | DATE | G03/13/24 | SIGNED: | JOB NO: 22050 | SHEET | T-1 | Robert W. Fulp | Fe No. 446/14 | Or | T-14

PROJECT SITE -

SECTION 13, TOWNSHIP 22 SOUTH, RANGE 17 EAST HERNANDO COUNTY, FLORIDA FDOT HIGHWAY SEGMENT #08-020-000 U.S. 19 MILEPOST (M.P. 10.635) ACCESS CLASSIFICATION 3 POSTED SPEED: 60mph

VICINITY MAP

TABULATION OF QUANTITIES

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	(PURCHASED BY CONTRACTOR FROM POWER CO.)																											
39-2-1	ELECTRICAL SERVICE WIRE (F&I)	LF																									100	-
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5-4-11	LIGHT POLE COMPLETE (F&I)(STD. FND.)(30' MH)	EA										-	-			1								-		-		
5-4-13	LIGHT POLE COMPLETE (F&I)(STD. FND.)(40' MH)	EA																						-			1	
5-7-11	LOAD CENTER (F&)(SECONDARY VOLTAGE)	EA		+-	-	-										-								_			11	
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s, LLC

- THE CONTRACTOR SHALL MEET THE CRITERIA OUTLINED IN FDOT's "THE MINIMUM SPECIFICATIONS FOR TRAFFIC CONTROL SIGNAL DEVICES" (MSTCSD).
- THE LOCATIONS OF UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE. PRIOR TO DIGGING, THE CONTRACTOR SHALL DETERMINE THE EXACT LOCATIONS OF ALL UTILITIES.
- 3. ONE WEEK PRIOR TO THE START OF THE TRAFFIC SIGNAL INSTALLATION, THE SIGNAL CONTRACTOR SHALL CONTACT:

a) DWAYNE DEMPSEY

#JOVATINE DEMPSET TRAFFIC OPERATION CONSTRUCTION LIAISON FLORIDA DEPARTMENT OF TRANSPORTATION 11201 NORTH McKINLEY DRIVE TAMPA, FLORIDA 33612 PHONE: (813) 975-6269 E-MALL: cuttle, dempsey@dot, sinte fl.us

b) J. SCOTT HERRING, P.E. HERNANDO COUNTY DIRECTOR OF PUBLIC WORKS AND COUNTY ENGINEER 1525 EAST JEFFERSON STREET BROOKSVILLE, FLORIDA 34801 PHONE: 3521 754-4002

- 4. PRIOR TO ALL TRAFFIC SIGNAL CHANGES (I.E. MAINTENANCE OF TRAFFIC CHANGES, NEW SIGNAL INSTALLATIONS, DIFFERENT PAYEMENT CONFIGURATIONS, ETC.) CONTACT RICK NAPORA, F.D.O.T. DISTRICT SEVEN TRAFFIC OPERATIONS, AT (813) 615-8620, AT LEAST 48 HOURS IN ADVANCE FOR MPLEMENTATION OF APPROPRIATE SIGNAL TIMMOS.
- 5. THE CONTRACTOR SHALL CONTACT THE LOCAL POWER COMPANY FOR THEIR ASSISTANCE IN PERFORMING ALL NECESSARY WORK UNDER POWER LINES. SUCH WORK SHALL INCLUDE, BUT NOT BE LIMITED TO THE INSTALLATION OF SPAN WIRE, SIGNAL CABLE, FIBERGIASS INSULATORS, AND / OR SIGNAL POLES
- 8. UNLESS OTHERWISE NOTED, ALL REMOVED EQUIPMENT SHALL BE TURNED OVER TO THE HERMANDO COUNTY SIGNAL SHOP, IN EXISTING CONDITION, EXCEPT FOR POLES AND SPAN WIRES, WHICH SHALL BE DISPOSED OF BY THE CONTRACTOR IN AREAS PROVIDED BY HIM.
- 7. THE CONTRACTOR SHALL SUBMIT THE AS-BUILT PLANS TO THE ENGINEER FOR APPROVAL PRIOR TO THE 48 HOUR TEST, UPON THE 48 HOUR TEST, THE ENGINEER SHALL PROVIDE AT LEAST ONE COPY OF THE FINAL AS-BUILT PLANS TO HERNANDO COUNTY AND ONE COPY TO DISTRICT TRAFFIC OPERATIONS
- 8. THE CONTRACTOR SHALL FIELD VERIFY ALL CRITICAL ELEVATIONS PRIOR TO ORDERING SIGNAL STRAIN POLES AND MAST ARMS.
- 9. THE TYPE OF EQUIPMENT USED IN THE INSTALLATION OF MAST ARMSFOUNDATIONS, OVERHEAD/CANTILEVER SIGNS/FOUNDATIONS, AND THE MOVEMENT/INSTALLATION OF STRAIN POLES SHALL MEET THE FOLLOWING REQUIREMENTS:

a) OVERHEAD LINES SHALL STAY IN PLACE BOTH VERTICALLY AND HORIZONTALLY

b) CONTRACTOR SHALL MEET ALL APPLICABLE OSHA REQUIREMENTS. ANY COST ASSOCIATED WITH THE TYPE OF EQUIPMENT REQUIRED FOR THIS INSTALLATION IS INCLUDED IN THE RELATED PAY ITEMS.

- ALL GROUND ROD CONNECTIONS SHALL BE MADE USING EXOTHERMIC WELD GROUND CONNECTORS.
- ALL UNUSED REDS SHALL BE HARD WIRED TO 120V AC, UNLESS A FLORIDA DEPARTMENT OF TRANSPORTATION APPROVED PLUG-IN JUMPER IS USED.
- CONTACT DWAYNE DEMPSEY AT (813) 323-1159 WITH THE FDOT TRAFFIC OPERATIONS PRIOR TO PERFORMING THE 48-HOUR BURN-IN PERIOD TEST PER FDOT STADARD SPECIFICATION 811-4.
- A MANDATORY PER-SIGNALIZATION MEETING SHALL BE HELD PRIOR TO START OF PROJECT. ALL MATERIAL SUBMITTALS SHALL BE PROVIDED AT THE MEETING OR PRIOR TO THE MEETING.
- PLEASE CONTACT JAMES MEYER 48 HOURS PRIOR TO WORKING IN FDOT RW AT (362) 848-2810. TO ACTIVATE PERMIT AND SCHEDULE PRE-CONSTRUCTION MEETING.
- CONTRACTOR SHALL COMPLY WITH OSHA'S STANDARDS 29 CFR PART 1926, SUBPART OC FOR VERTICAL AND HORIZONTAL CLEARANCES TO THE OVEREAD DISTRIBUTION AND TRANSMISSION POWER LINES.

PAY ITEM FOOTNOTES

630-2-11

ALL CONDUIT BUNS SHOWN ON THE PLANS ARE SCHEMATIC AND FIELD ADJUSTMENTS MAY BE NECESSARY WITH THE EXCEPTION OF ELECTRICAL POWER SERVICE DUCTS, AND JACK AND BORE SLEEVES, ALL UNDERGROUND AND UNDER PAYEMENT CONDUITS SHALL BE SCHEDULE 40 PVC WITH A MINIMUM SIZE OF TWO INCHES, UNLESS OTHERWISE SPECIFIED IN THE PLANS. TWO SEPARATE UNDERGROUND CONDUIT RUNS LOCATED 180 DEGREES APART ARE REQUIRED FOR ALL SIGNAL STRAIN POLES AND MAST ARMS. THERE SHALL BE MINIMO FT TWO INCH CONDUIT BETWEEN THE LAST LOW VOLTAGE (LOOPS) PULL BOX LOCATED NEAR THE CONTROLLER CABINET AND THE CONTROLLER CABINET.

830-2-12:

CONDUITS INSTALLED WITH THE DIRECTIONAL BORE METHOD SHALL BE HDPE WITH A MINIMUM SIZE OF TWO INCHES UNLESS OTHERWISE NOTED IN THE PLANS.

ALL FOUR-INCH HDPE OUTERDUCT CONDUIT INSTALLED WITH DIRECTIONAL BORE METHOD SHALL INCLUDE THREE RUNS OF ONE INCH HDPE INNERDUCT CONDUITS SHALL BE INCLUDED IN PAY ITEM 830-2-12.

632-7-1

EACH PHASE/MOVEMENT SHALL BE WIRED FROM THE SIGNAL DISPLAY TO THE CONTROLLER AS A SEPARATE PHASE/MOVEMENT. THIS INCLUDES THE LEFT TURN MOVEMENT WHICH SHALL HAVE CONDUCTORS AVAILABLE FOR EITHER PROTECTED OR PERMISSIVE MOVEMENTS. THE CONTRACTOR SHALL VERHY COLOR CODES FOR SIGNAL CABLE AND INTERCONNECT CABLE WITH THE HERNANDO COUNTY SIGNAL SHOP BEFORE ORDERING, AND WIRE THE SIGNAL IN ACCORDANCE WITH THAT COLOR CODE AND F.D.O.T.SPECIFICATIONS. THERE SHALL BE ONE NEUTRAL PER APPROACH.

632-7-1:

ALL TRAFFIC SIGNAL FIXTURES SHALL BE WIRED AND PHASED PER THE INTERSECTION WIRING DIAGRAM PROVIDED BY HERWANDO COUNTY TRAFFIC OPERATIONS. CABLE SIZE SHALL BE AS CALLED FOR IN THE WIRING DIAGRAM. ALL SIGNAL CABLE WIRING SHALL BE CONCALED WITHIN THE ARM. MOUNTING BRACKET AND FIXTURE; NO EXPOSED WIRING SHALL BE PREMITTED. A MINIMUM OF THREE (3) FEET OF SLACK CABLE SHALL BE PROVIDED AT ALL HAND HOLES. ALL SIGNAL CABLES ON SPAN WIRES SHALL BE ATTACHED TO THE MESSANGER BY USE OF LASHING RODS IN PLACE OF THE WRAPS.

632-7-1:

ALL SPARE CONDUCTORS IN TRAFFIC SIGNAL FIXTURES SHALL BE CONNECTED TO AC: IN THE FIXTURE. ALL SPARE CONDUCTORS IN THE CABINET SHALL BE CONNECTED TO THE GROUND BUSS BAR.

639-1-122

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE COMPANY PROVIDING ELECTRICAL POWER TO DETERMINE IF A SERVICE PROCESSING OR CONNECTION FEE IS REQUIRED. THE COST OF ANY SUCH FEE SHALL BE INCLUDED AS PART OF PAYMENT FOR ELECTRICAL POWER SERVICE ASSEMBLY.

639-1-122:

THE COST OF THIS ITEM SHALL INCLUDE THE COST FOR THE APPROPRIATE METER CAN CONTAINING AN INTERNAL BYPASS SHUNT SWITCH AS REQUIRED BY THE LOCAL POWER COMPANY. THE CONTRACTOR SHALL VERRY THE TYPE OF METER CAN AND A BREAKER BOX TO USE WITH THE HERNANDO COUNTY SIGNAL SHOP. A SEPARATE CIRCUIT BREAKER SHALL BE ADDED TO THE SERVICE POLE FOR THE LUMINAIRES ATTACHED TO THE SIGNAL POLES. IN ADDITION, WHER ALL LUMINAIRES TO ONE (1) PHOTO ELECTRIC CELL ATTACHED TO THE SERVICE POLE.

639-2-1:

THE PAYMENT OF THIS ITEM SHALL BE BASED ON THE LINEAR FOOT OF A COMPLETE WIRE (I.E. ALL CONDUCTORS).

646-1-11:

ALUMINUM PEDEBTALS SHALL BE MOUNTED ON TRANSFORMER BASE.

649-21-21: AT LOCATIONS WHERE UNDERGROUND LITLITIES ARE IN CLOSE PROXIMITY TO THE SIGNAL POLE FOUNDATION. AS

DETERMINED BY THE CONTRACTOR, THE CONTRACTOR SHALL HAND DIG THE FIRST FOUR FEET OF THE HOLE FOR THE POLE FOUNDATION.

649-21-21:

ALL SPLICES IN THE POLE BASES SHALL BE MADE USING 3M 3878G CONNECTOR SEALING PACKS OR EQUAL, ALL SPLICES SHALL BE MADE AT THE HAND HOLE. SPLICES SHALL BE MADE PER HERNANDO COUNTY STANDARD SPLICING DETAILS. A MINIMUM OF THREE 3J FEET OF SLACK CABLE SHALL BE PROVIDED AT ALL HAND HOLES

649-21-21:

ALL SIGNAL POLE FOUNDATIONS SHALL HAVE A MINIMUM OF THREE 2" AND ONE 1" PVC CONDUITS. UNUSED CONDUITS SHALL BE RUN TO THE NEAREST SIGNAL PULL BOX OR SIGNAL CABINET, WHICHEVER IS CLOSEST.

653-1-11:

ALL WALK AND DON'T WALK PEDESTRIAN SIGNALS SHALL BE LED COUNT-DOWN PER FDOT SPECIFICATIONS WITH A SOLID SYMBOL DISPLAY.

660-4-11 & 660-4-12:

THIS ITEM SHALL INCLUDE ALL LABOR AND MATERIALS REQUIRED TO FURNISH AND INSTALL A VIDEO DETECTION SYSTEM. THE DETECTION SYSTEM SHALL BE COMPATABLE WITH THE HERNANDO COUNTY'S CONTROLLER EQUIPMENT, CABINET AND TRAFFIC SIGNAL COMPUTER SYSTEM. THE CONTRACTOR SHALL COORDINATE WITH THE MAINTAINING AGENCY FOR UNIT ACCEPTANCE AND SHALL BE LISTED IN THE F.D.O.T. APPROVED PRODUCT LIST. THE VIDEO DETECTION SYSTEM SHALL BE VISION HID CAMERAS. CONTACT CONNIE BRAITHWAITE AT ECONOLITE CONTROL PRODUCTS: (904) 758-0745.

PAY ITEM FOOTNOTES

665-1-11:

SHALL BE A MECHANICAL TYPE PUSHBUTTON (NO LED) AND SHALL INCLUDE ADDITIONAL COST OF LABOR AND MATERIALS REQUIRED FOR INSTALLATION OF PEDESTRIAN SIGNAL SIGN FTP-68B-06.

670-5-11

ALL FIELD WIRES IN CABINET SHALL BE IDENTIFIED WITH PANDUIT #PLF1M-C MARKER TIES; OR EQUIVALENT.

870-5-111

ALL CONTROLLER CABINET BASES SHALL HAVE ONE 4-INCH CONDUIT FOR FUTURE FIBER OPTICS COMMUNICATIONS. IT SHALL BE STUBBED OUT TO A FIBER OPTICS PULL BOX.

870-5-111:

PRIOR TO SIGNAL CABINET ASSEMBLY INSTALLATION, THE SIGNAL CONTRACTOR SHALL VERIFY THE OPERATION OF THE SIGNAL EQUIPMENT BY DERFORMING A BURN-IN OF THE ASSEMBLY FOR EACH LOCATION WITHIN THE PROJECT. A REPRESENTATIVE FROM THE FOOT TRAFFIC OPERATIONS WILL OBSERVE AND ATTEST TO THE VERIFICATION. THIS BURN-IN WILL BE AT A DESIGNATED LOCATION BUPPLIED BY THE SIGNAL CONTRACTOR.

70-5-111:

ENSURE THAT ALL TRAFFIC SIGNAL CONTROLLERS ARE FULLY COMPATIBLE WITH THE HEENANDO COUNTY SIGNAL SYSTEM. THE COUNTY'S CURRENT SYSTEM COMPRISED OF COBALT CONTROLLER WITH AMB DATA KEY AND RECEPTACLE. PROVIDE FOOT TYPE-V TRAFFIC SIGNAL CABINETS WITH A PHOTO-CELL, RACK MOUNT DETECTORS, ALL INTERFACE PANELS INCLUDING PEDESTRIAN ISOLATION PANEL, ETHERNET SWITCH AND 18 IN. ALUMINUM CABINET RISER; (FROM CABINET MANIPACTURE), ALL CABLING NOT INCLUDEING CONNECTION TO THE PATCH PANELS, NECESSARY TO CONNECT THE ETHERNET SWITCH TO ALL ITEEMS INCLUDING THE UPS, VIDEO DETECTION AND CONFLICT MONTON. THE UPS SHALL BE MOUNTED ON A SEPARATE CONCRETE SLAB. NOTIFY HERNANDO COUNTY SIGNAL SHOP AND FODT DISTRICT-T TRAFFIC OPERATIONS VM PHONE ATLEAST 48 HOURS IN ADVANCE OF TURNING ON A NEW OR MODIFIED CONTROLLER CABINET ASSEMBLY. CABINET DOOR SHOULD O'PED NAMY FROM TRAFFIC, INCLUDE AN BIGHT PHASE BACKPLANE WITH MINIMUM OF 19 POSITIONS ON THE LOAD BAY IN THE CABINET. COORDINATE WITH HERNANDO COUNTY SIGNAL SHOP FOR ACCEPTABLE IN TIS OWN CONDUIT, HERNANDO COUNTY CURRENTLY UTILIZES ECONOLITE TS-2 TYPE-1 CONTROLLER

682-1-113:

THE COTY CAMERA - CONTACT AND COORDINATE WITH HERNANDO COUNTY FOR ACCEPTABLE CHOICES OF CAMERAS.

4-1-1:

THE ETHERNET SWITCH SHALL BE A ITS 8012 WITH (4) 100001x8FP SINGLE MODE, I.C., 1550 MM, 25KM TRANSCEIVERS, CONFIRM MAKE AND MODEL WITH HERMANDO COUNTY DEPARTMENT OF PUBLIC WORKS PRIOR TO SUBMITTING SHOP DRAWMOS, PAY ITEM TO INCLUDE ALL APPROPRIATE PAIC CORDS AND CABLING NECESSARY CONNECTIONS TO ALL EQUIPMENT INSIDE THE CONTROLLER CABINET.

685-1-13:

THE "UNINTERRUPTED POWER SUPPLY (UPS) UNIT SHALL BE HOUSED IN A SEPARATE ENCLOSED CABINET OR IN A TRAFFIC SIGNAL CABINET WHICH IS DESIGNED TO HOUSE THE UPS ASSEMBLY IN ADDITION TO THE TRAFFIC SIGNAL CONTROL EQUIPMENT, IF A SEPARATE CABINET IS REWIRED, THIS PAY ITEM SHALL INCLUDE CABINET TO HOUSE THE UPS ASSEMBLY. THE TRAFFIC SIGNAL CABINET CONCRETE BASE SHALL BE LARGE ENOUGH TO SUPPORT THE TRAFFIC SIGNAL CABINET AND THE UPS CABINET. PAYMENT FOR ADDITIONAL CONCRETE BASE, INCLUDING LABOR AND MATERIAL, SHALL BE INCLUDED IN TRAFFIC CONTROLLER ASSEMBLY PAY ITEM NUMBER. THE CONTRACTOR SHALL SCONTROLLER ASSEMBLY PAY ITEM SUMBER. THE CONTRACTOR SHALL SEE OF THE SAME MAKE AND MODEL AS IS CURRENTLY BEING USED BY THE MANITANING AGENCY.

685-1-13:

THE TOP ELEVATION OF THE CONTROLLER CABINET BASE AND THE TECHNICIAN SERVICE PAD SHALL BE A MINIMUM OF 4 INCHES ABOYE GROUND FINISHED GRADE. THE BASE SHALL BE INTEGRATED WITH THE UPS AS A SINGLE BASE FOR MOUNTING OF BOTH THE CONTROLLER CABINET AND UPS. THE CONTROLLER CABINET SHALL HAVE AN EXTENSION RISER TO BRING BOTTOM OF THE CABINET TO THE CROWN IN THE ROAD. THE UPS REPLACEMENT SHALL BE COORDINATED WITH HERNANDO COUNTY TRAFFIC OPERATIONS. THERE SHALL BE A MINIMUM OF TWO (2) FEET SEPARATION BETWEEN THE CONTROLLER CABINET AND THE UPS CABINET.

685-1-13:

LIPS - HOUSE AN ALPHA FX 1100 OR APPROVED EQUAL UNINTERRUPTED POWER SUPPLY UNIT IN AN ALLIMINUM NEAM AX ENCLOSURE MOUNTED ON A SEPARATE CONCRETE PAD, INCLUDE THE FOLLOWING 1100 WATT POWER MODULE WITH SMMP ETHERNET CONNECTION THAT MEETS UL, 1775, FOUR BATTERIES AND GENERATOR CORD ACCESS COMPARTMENT, INSTALL ONE 2 IN. AND ONE 1 IN. CONDUITS TO THE TRAFFIC CABINET FOUNDATION, INSTALL A NETWORK CABLE THROUGH THE ONE IN. CONDUIT FOR REMOTE ACCESS, COORDINATE WITH HERNANDO COUNTY TRAFFIC OPERATIONS FOR SPECIFICATIONS PRIOR TO ORDERING.

00-3-201:

SHALL INCLUDE PROPERLY DESIGNED AND SIZED ADJUSTABLE HANGERS, BRACKETS, CLAMPS, AND ALL MISCELLANEOUS HARDWARE NECESSARY TO INSTALL THE SIGNS AS SHOWN IN THE PLANS.

700-5-22

SHALL BE EDGE LIT L.E.D. TYPE AND SHALL BE LISTED IN THE F.D.O.T APPROVED PRODUCT LIST.

60% PLANS
NOT FOR
CONSTRUCTION
12/20/24

LINCKS & ASSOCIATES, 5023 West Loural Street Tampa, Florida 33607 Tel (613) 2899-0039

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BIVG. DESIGNED OAR/FINF

SIGNALIZATION PLANS
19 (Commercial Way) & Glen Lakes E
GENERAL NOTES

REMICONS U.S.

REMEION

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ALL WORK PERFORMED IN THE DEPARTMENT'S RIGHT OF WAY SHALL BE DONE IN ACCORDANCE WITH THE MOST CURRENT EDITIONS OF:

- A) FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION.
- B) FDOT DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY OPERATIONS ON THE STATE HIGHWAY SYSTEM.
- C) FDOT DESIGN MANUAL (FDM)
- D) FDOT FLEXIBLE PAVEMENT DESIGN MANUAL FOR NEW CONSTRUCTION AND PAVEMENT REHABILITATION.
- I) ALL AREAS IN THE FDOT RIGHT OF WAY DISTURBED DURING CONSTRUCTION SHALL BE RESTORED, COMPACTED. SODDED AND WATERED IN CONFORMANCE WITH THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 570 AND 981 AND DESIGN STANDARDS INDEX 570-010.
- 2) DAYTIME WORK ONLY. THE FDOT ENGINEER MUST APPROVE NIGHT WORK AND LANE CLOSURES AT LEAST 48 HOURS IN ADVANCE. THE APPLICANT MUST NOTIFY THE FLORIDA DEPARTMENT OF TRANSPORTATION LOCAL MAINTENANCE OFFICE 48 HOURS IN ADVANCE OF STARTING ANY PROPOSED WORK, CALL: BROOKSVILLE MAINTENANCE, James Meyer @ 352-848-2610
- 3) ALL TRAFFIC STRIPES AND PAVEMENT MARKINGS ARE TO BE LEAD-FREE, NON-SOLVENT BASED THERMOPLASTIC. THE PERMITTEE SHALL FURNISH THE DEPARTMENT WITH THE MANUFACTURER'S CERTIFICATION THAT THE THERMOPLASTIC IS 'LEAD FREE'. REMOVAL OF EXISTING STRIPING SHALL BE DONE BY MILLING AND RESURFACING OF THE FRICTION/SURFACE COURSE TO OBLITERATE OBSOLETE PAVEMENT MARKINGS.
- 4) ALL PROPOSED TRAFFIC SIGNS AND PAVEMENT MARKINGS SHALL BE IN CONFORMANCE WITH AND CONSTRUCTED TO THE FDOT DESIGN STANDARDS INDEX SERIES 600 AND 700 SERIES. THE PROPERTY OWNER MUST MAINTAIN THE TRAFFIC SIGNS AND MARKINGS FOR THE DRIVEWAY.
- 5) THE FDOT REQUIRES DOCUMENTATION FOR SUCCESSFUL COMPLETION OF AN APPROVED WORK ZONE TRAFFIC CONTROL TRAINING COURSE FOR THE AGENCY. UTILITY OR CONTRACTOR EMPLOYEE(S) INSTALLING AND/OR MAINTAINING THE APPROVED MAINTENANCE OF TRAFFIC PLAN. DOCUMENTATION IS TO BE FURNISHED TO THE DEPARTMENT AT THE PRE-CONSTRUCTION MEETING OR BEFORE OCCUPYING STATE RIGHT-OF-WAY
- 6) MAINTENANCE OF TRAFFIC PLAN FOR WORK ZONES SHALL BE IN CONFORMANCE WITH ALL APPLICABLE INDICES OF THE FDOT DESIGN STANDARDS INDEX 102 SERIES ACCORDING TO THE TYPE OF ROADWAY AND TYPE OF WORK BEING PERFORMED.
- 7) SILT FENCE FOR EROSION CONTROL IN CONFORMANCE WITH THE FLORIDA EROSION SEDIMENT CONTROL MANUAL IS REQUIRED FOR ALL AREAS OF CONSTRUCTION WITHIN FDOT RW, PERPENDICULAR TO AND PARALLEL WITH THE
- 8) SIDEWALK SHALL BE CONSTRUCTED PER FDOT INDEX 522-001 AND 522-002. DETECTABLE WARNING STRIPS. A.K.A. TRUNCATED DOMES, FOR DISTRICT SEVEN CONSTRUCTION AND MAINTENANCE SHOULD BE EITHER INSET CERAMIC TILES OR THERMOPLASTIC DETECTABLE WARNING STRIPS. THESE WARNING SURFACES SHALL ONLY BE PROVIDED BY THE FOLLOWING VENDORS/MANUFACTURERS.
- A) INLINE TRUNCATED DOME EZ TILE SUPPLIED BY PROFESSIONAL PAVEMENT **PRODUCTS**
- B) TOPMARK SUPPLIED BY FLINT TRADING
- C) VANGUARD TRUNCATED DOME SUPPLIED BY VANGUARD
- 9) MAINTENANCE OF TRAFFIC PLAN MUST INCLUDE FDOT DESIGN STANDARDS INDEX 102-860 PEDESTRIAN CONTROL FOR CLOSURE OF SIDEWALKS.
- 10) FDOT REQUIRES A PIPE VIDEO BE DONE ON ALL PIPES ASSOCIATED WITH A PERMIT PROJECT IN FDOT ROW. SPEC 430-4.8.
- 11) PROVIDE C.E.I. INSPECTION PER FDOT SPECIFICATION 105 "CONTRACTOR QUALITY CONTROL GENERAL REQUIREMENTS".

UTILITY / AGENCY OWNERS

HENRY ICLOBUCAR ZAYO GROUP / FORMERLY 130 N MAIN ST

ALLEN TURNER 352-540-6219 HERNANDO COUNTY UTILITY DEPT. BROOKSVILLE, FL 34801

NETWORK RELATIONS 877-386-8344 Ext 2 CENTURY LINK 1025 EL DORADO BLVD BROOMFIELD, CO 80021

DINO FARRUGGIO 561-663-2729 ATAT / DISTRIBUTION 6628 LAKESIDE RD. WEST PALM BEACH, FL 33411 DON TAULBEE 352-506-4000 Fyt 3139 WITHLACOOCHEE RIVER ELECTRIC COOPERATIVE 10006 CORTEZ BLVD. WEEK WACHEE, FL 34613

C

Blvd.

SIGNALIZATION PLANS (Commercial Way) & Glen Lakes 19 S

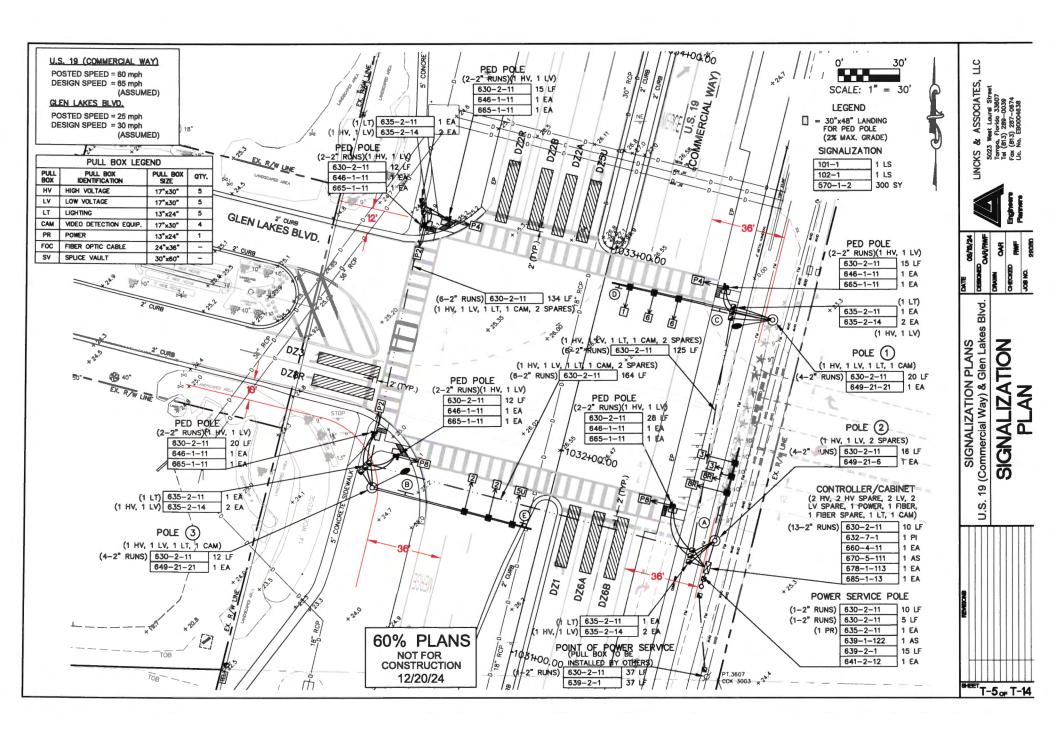
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PRIOR TO CONSTRUCTION **48 HOURS BEFORE DIGGING**

CALL **TOLL FREE** 1-800-432-4770

SUNSHINE STATE ONE CALL CENTER

60% PLANS NOT FOR CONSTRUCTION 12/20/24

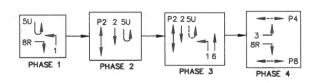


- . THE MAJOR STREET IS US 19 (COMMERCIAL WAY), PHASE 3 (MOVEMENTS 2 AND 6). THE MINOR STREET IS GLEN LAKES BLVD., PHASE 4 (MOVEMENT 3 AND 8).
- 1. THE CONTROLLER CABINET SHALL BE CAPABLE OF RUNNING A SOP 10. THE INITIAL SOP SHALL BE SOP 7 (MODIFIED), AS SHOWN, WITH THE FOLLOWING:
 - (A) FLASHING OPERATION IS YELLOW FOR MOVEMENTS 2 AND 6 AND RED FOR ALL OTHER MOVEMENTS.
 - (B) COORDINATION ON PHASE 3 (MOVEMENTS 2 AND 6).
 - (C) REFER TO GENERAL NOTES SHEET FOR ADDITIONAL CONTROLLER REQUIREMENTS.
 - (D) SIGNAL TIMING MAY REQUIRE FIELD ADJUSTMENTS BY THE COUNTY ENGINEER.

ENERAL NOTES

- THE POLE LOCATIONS (STATION / OFFSET) ARE MEASURED FROM THE BASELINE OF SURVEY ALONG U.S. 19 (COMMERCIAL WAY).
- ALL VEHICULAR SIGNAL HEADS SHALL HAVE BACKPLATES WITH REFLECTIVE BORDERS AND TUNNEL VISORS.
- POWER SERVICE METER BASE AND DISCONNECT SHALL BE INSTALLED ON THE CONCRETE SERVICE POLE AS SHOWN ON THE PLANS AND PER INDEX NUMBERS 17504 AND 17736.
- THE CCTV CAMERA SHALL BE MOUNTED TO THE TOP OF POLE #2 USING A 10 FT. LONG SIGN BRACKET ARM. ANY NECESSARY CABLE SUPPORTS REQUIRED WITHIN THE SIGNAL POLE SHALL BE INCIDENTAL TO THE COST OF THE SIGNAL POLE. THE CCTV CAMERA CABLES SHALL RUN TO THE CONTROLLER CABINET THROUGH A SEPARATE PULL BOX AND CONDUIT AS SHOWN ON PLANS. THE CABINET SHALL BE WIRED TO ACCOMMODATE THE CCTV CAMERA.
- ONE (1) DISCONNECT WITH TWO (2) BREAKERS (ONE LABELED SIGNALS AND ONE LABELED LIGHTING) SHALL BE INSTALLED ON THE POWER SERVICE POLE AS INDICATED ON THE PLANS.

S.O.P. 7 MODIFIED



DETECTION DEVICES

V 660-4-12

VEHICULAR SIGNAL HEAD DETAILS

PEDESTRIAN SIGNAL HEAD DETAILS











PED. SIGNAL COUNT-DOWN 1-SECT., 1-WAY 6 AS 653-1-11

P2

P4

P8

LED

POLE LOCATIONS

	_		
POLE NO.	(1)	(2)	(3)
STATION	1032+85.70	1031+75,70	1031+60.55
OFFSET	83.4° RT	82,0' RT	84.0' LT
ELEV. AT GROUND	22.30	24.45	24.45
ELEV. AT TOP FOUNDATION	22.80	24,95	24.95
HIGHEST ELEVATION OF ROADWAY BENEATH SPAN WIR	26,55 E	22.60	26.25







2

6

R \odot





1 AS 650-1-14 LED

1 A5
650-1-19
LED









R10-3e 2 REQUIRED 4 REQUIRED

VIDEO VEHICLE DETECTION ASSIGNMENTS

CAMERA DETECTOR	DETECTION ZONE	DETECTION SIZE	CONNECT TO MOVEMENT	DELAY TIME (SEC.)
CD1	DZ2R	6'x30'	2	-
CD1	DZ2A	6'x30'	2	-
CD1	DZ2B	6'x30'	2	-
CD1	DZ5U	6'x30'	5	-
CD2	DZ3	6'x30'	3	-
CD2	DZ8R	6'x30'	8	-
CD2	DZ6A	6'x30'	6	-
CD2	DZ6B	6'x30'	6	-
CD2	DZ1	6'x30'	6	-
				-

CONTROLLER TIMINGS								
MOVEMENT NUMBER	1	2	3	4	5	6	7	8
MINIMUM GREEN	7	20	7	-	7	20	-	7
EXTENSION	3	3	3	-	3	3	-	3
MAXIMUM GREEN 1	TO BE DETERMINED							
MAXIMUM GREEN 2	1	BY	HERI					
YELLOW CLEARANCE	5.5	5.5	5.5	-	5.5	5.5	-	5.5
ALL RED	2.0	2.7	2.5	-	2.0	2.0	-	2.5
PEDESTRIAN WALK	-	7	_	7	-	-	_	7
PED. CLEARANCE	-	38	-	38	-	-	-	38
RECALL	T-	MIN	-	-	-	MIN	-	-

SIGN DETAILS

SIGN (A) SIGN (B) Glen Lakes Commercial XXXXX YAW XXXXX BLVD XXXXX 700-5-22 1 EA 700-5-22 1 EA

SIGN (C) Glen Lakes XXXX BLVD

700-5-22 1 EA

U-TURN YIELD TO RIGHT TURN

SIGN (D)

(R10-16) 30"x36" 700-3-201 1 EA

ONLY FTP-82-08 24"x36" 700-3-201

SIGN (E)

1 EA

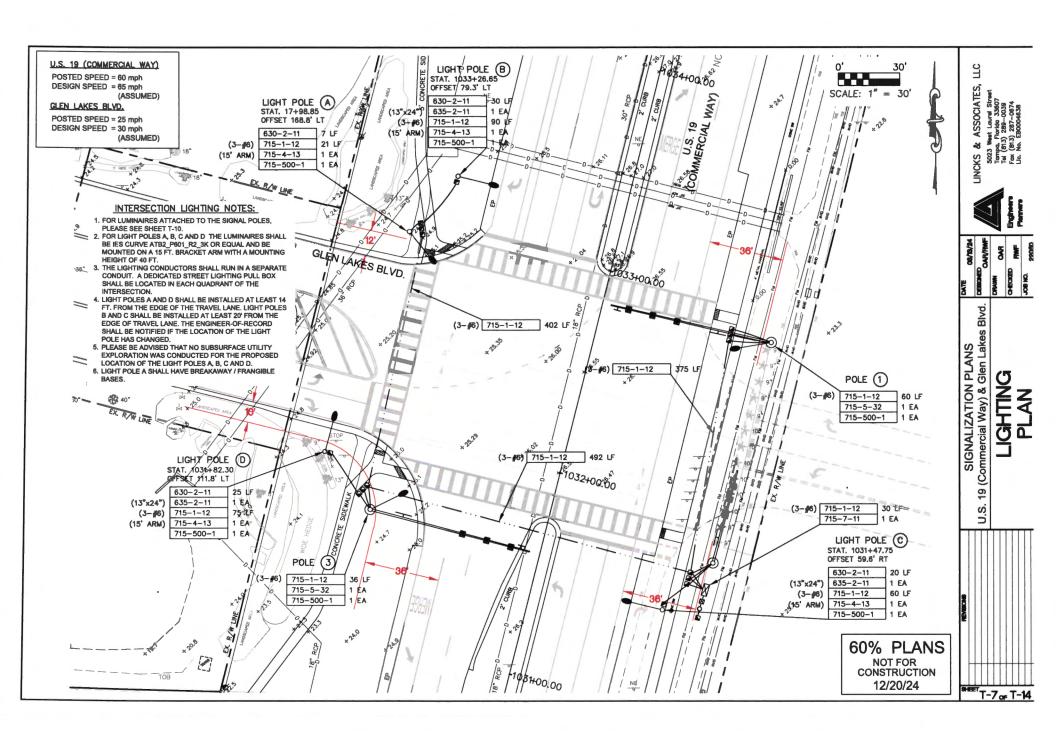
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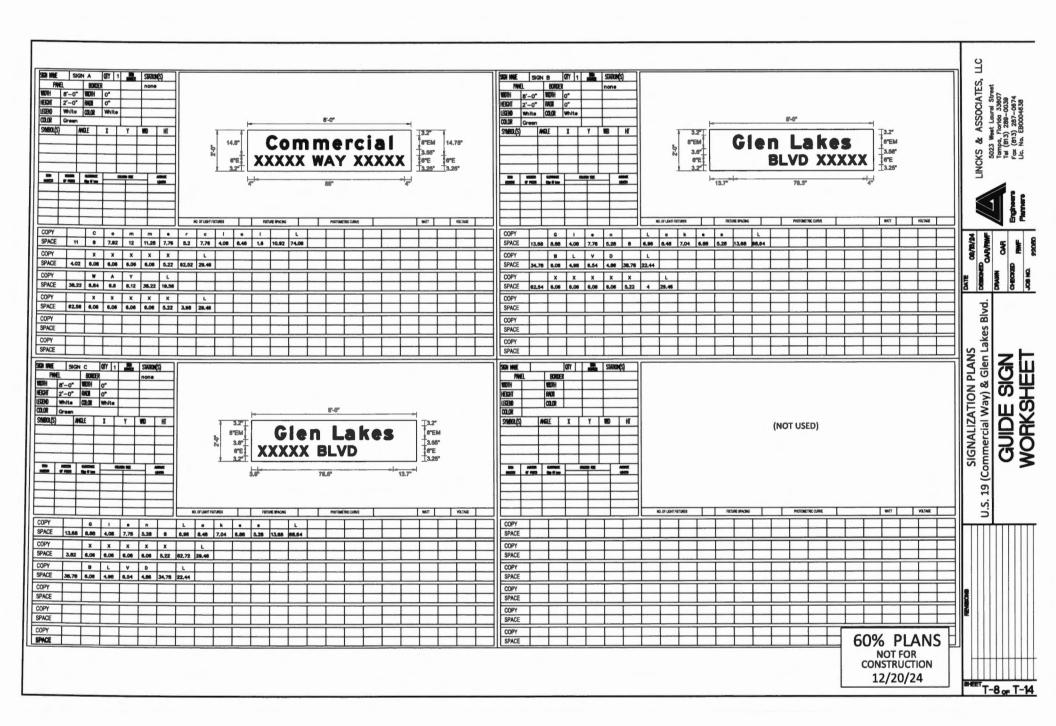
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SIGNALIZATION PLANS (Commercial Way) & Glen Lakes 19 S





T-9 or T-

(AINANO TERRO ENIANVEA) GUIDE SIGN WORKSHEET

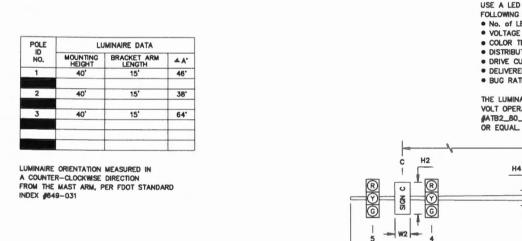
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Fox (813) 289-0039
Fox (813) 289-0054

60% PLANS
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12/20/24

THIS SHEET NOT USED (SEE ROADWAY PLANS)



BACK PED. PLATES SIGNAL Y/N

SIGNAL V/H

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SHEET

O. NO.

T-5/6

LOCATION

1032+85.70

T-5/6 1031+60.55 24.95

1031+75.70 24.95

BY STA.

ARM NO. CROWN

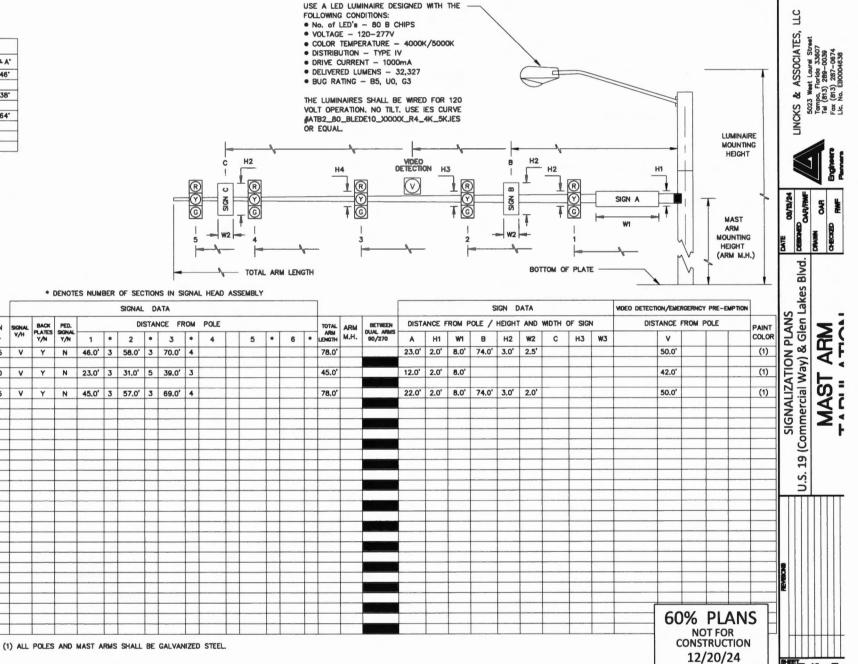
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ELEV.

26,55

22.60

26.25



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TABLE NOTES

1. ASSEMBLY NUMBER LEGEND

SINGLE ARM

ARM TYPE -POLE TYPE = D# - S#

= E# - T#

= F# - W#

DOUBLE ARM:

FIRST ARM TYPE - SECOND ARM TYPE - POLE TYPE * D# - D# - S#

E# - E# - T#

F# - F# - W#

- 2. IF AN ENTRY APPEARS IN COLUMNS "FAA" AND "FBA", A SHORTER ARM IS REQUIRED. THIS IS OBTAINED BY REGIVING LENGTH FROM THE ARM TIP. FOR THESE CASES THE MAST ARM LENGTH SHALL BE SHORTENED FROM "FA" TO "FAA" AND THE TIP DIAMETER SHALL BE INCREASED FROM "FB" TO "FBA".
- 3. IF AN ENTRY APPEARS IN COLUMNS "UAA" AND "UCA", A SHORTER POLE IS REQUIRED. THIS IS OBTAINED BY REMOVING LENGTH FROM THE POLE TIP. FOR THESE CASES THE POLE HEIGHT SHALL BE SHORTENED FROM "UA" TO "UAA" AND THE POLE TIP DIAMETER SHALL BE INCREASED FROM "UC" TO "UCA".
- 4. THE FOUNDATIONS FOR STANDARD MAST ARM ASSEMBLIES ARE PRE-DESIGNED AND ARE BASED UPON THE FOLLOWING CONSERVATIVE SOIL CRITERIA WHICH COVERS THE GREAT MAJORITY OF SOIL TYPES FOUND IN FLORIDA. ONLY COMPLETE THE "SPECIAL DRILLED SHAFT" DATA INFORMATION IF SITE CONDITIONS DICTATE DRILLED SHAFTS WITH ADDITIONAL FOUNDATION CAPACITY.

CLASSIFICATION

COHESIONIESS (FINE SAND)

FRICTION ANGLE

UNIT WEIGHT

30 DEGREES (30°) 50 LBS./CU.FT. (ASSUMED SATURDATED)

GENERAL NOTES:

- 1. WORK THIS SHEET WITH THE SIGNAL DESIGNER'S "MAST ARM TABULATION". SEE "MAST ARM TABULATION" FOR SPECIAL INSTRUCTIONS THAT INCLUDE NON-STANDARD HANDHOLE LOCATION, PAINT COLOR, TERMINAL COMPARTMENT REQUIREMENT, AND PEDESTRIAN FEATURES.
- WORK WITH INDEX NOS. 17743 AND 17745

TO BE COMPLETED FOR FINAL SUBMITTAL

> 60% PLANS NOT FOR CONSTRUCTION 12/20/24

SIGNALIZATION PLANS (Commercial Way) & Glen Lakes Blvd. 13 Si $\dot{}$

BY OTHERS

TO BE COMPLETED FOR FINAL SUBMITTAL

60% PLANS
NOT FOR
CONSTRUCTION
12/20/24

SIGNALIZATION PLANS
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

REPORT OF

CHECKED

DATE 08/19/24

DESIGNALIZATION PLANS
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

DATE 08/19/24

DESIGNALIZATION PLANS
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

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U.S. 19 (Commercial Way) & Glen Lakes Blvd.

DESIGNALIZATION PLANS
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

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GENERAL NOTES

- 1. ALL TRAFFIC CONTROL SHALL CONFORM TO FDOT STANDARD PLANS, INDEX 102-600 SERIES, AND THE FEDERAL HIGHWAY ADMINISTRATION'S MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CURRENT EDITION).
- 2. PRIOR TO COMMENCEMENT OF CONSTRUCTION OPERATIONS, INSTALL ADVANCE WARNING SIGNS, BARRICADES AND CONSTRUCTION SIGNS UTILIZING FDOT STANDARD PLANS, INDEX 600 SERIES.
- 3. UTILIZE STANDARD PLANS 102-600, 102-613, 102-615, and 102-660, AND OTHER INDEXES AS APPLICABLE FOR MAINTAINING TRAFFIC DURING CONSTRUCTION ACTIVITIES.
- 4. FOR WORK WITHIN FDOT RIGHT-OF-WAY, THE FDOT REQUIRES DOCUMENTATION FOR SUCCESSFUL COMPLETION OF AN APPROVED WORK ZONE TRAFFIC CONTROL TRAINING COURSE FOR THE AGENCY, UTILITY, OR CONTRACTOR EMPLOYEE(S) DESIGNING, INSTALLING, AND/OR MAINTAINING THE APPROVED MAINTENANCE OF TRAFFIC PLAN IN ACCORDANCE WITH DEPARTMENT PROCEDURE, TOPIC NO. 625-010-010.
- 5. FOR WORK WITHIN FDOT RIGHT-OF-WAY, THE CONTRACTOR SHALL PROVIDE A TEMPORARY TRAFFIC CONTROL PLAN PREPARED UNDER THE DIRECTION OF, AND SIGNED AND SEALED BY, A LICENSED FLORIDA PROFESSIONAL ENGINEER WHO IS EXPERIENCED IN PREPARING TRAFFIC CONTROL PLANS AND WHO IS CERTIFIED PER FDOT PROCEDURE. TOPIC NO. 625-010-010.
- 6. THE CONTRACTOR'S TRAFFIC CONTROL PLAN MUST RECEIVE THE APPROVAL OF THE FLORIDA DEPARTMENT OF TRANSPORTATION PRIOR TO IMPLEMENTION.
- 7. ALL TEMPORARY TRAFFIC CONTROL DEVICES FOR THE FOLLOWING FACILITIES SHALL BE DESIGNED AND INSTALLED TO MEET THE EXISTING POSTED SPEEDS AS STATED FOR ALL TRAFFIC CONTROL PHASES.
- 8. THE EXISTING POSTED SPEED LIMIT OF 60 MPH ALONG U.S. 19 (COMMERCIAL WAY) AND 25 MPH ALONG GLEN LAKES BOULEVARD SHALL BE MAINTAINED AT ALL TIMES DURING CONSTRUCTION.
- 9. ONLY SINGLE LANE CLOSURES WILL BE PERMITTED. LANE CLOSURES WILL NOT BE PERMITTED FROM THE HOURS OF 7:00 AM TO 9:00 PM AND FROM 4:00 PM TO 6:00 PM.
- 10. LANE CLOSURE RESTRICTIONS SHALL APPLY EQUALLY TO BOTH MOTOR VEHICLE AND BICYCLE TRAFFIC.
- 11. THE CONTRACTOR SHALL UTILIZE PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) DURING ACTIVE NIGHT-TIME OPERATIONS. THE DISPLAY SHALL BE PLACED IN ADVANCE OF THE FIRST WORK ZONE SIGN IN EACH DIRECTION APPROACHING THE WORK ZONE IN A LOCATION APPROVED BY THE ENGINEER. THE FOLLOWING PCMS DISPLAYS SHALL BE UTILIZED (SEE BELOW):



SIGNS TO BE LOCATED 800' IN ADVANCE OF CONSTRUCTION ZONE AND PLACED SEVEN DAYS PRIOR TO START OF CONSTRUCTION.



SIGNS TO BE LOCATED 500' IN ADVANCE OF CONSTRUCTION ZONE AND SHALL REMAIN FOR THE DURATION OF CONSTRUCTION.

60% PLANS NOT FOR CONSTRUCTION 12/20/24

TO BE COMPLETED FOR FINAL SUBMITTAL

60% PLANS
NOT FOR
CONSTRUCTION
12/20/24

SIGNALIZATION PLANS
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

CORNING CORRUPTION PLANS
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

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U.S. 19 (Commercial Way) & Glen Lakes Blvd.

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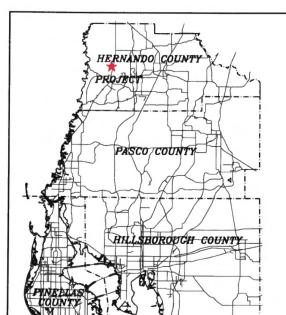
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CORRUPTION PLANS
U.S. 19 (Commercial Way) & Glen Lakes Blvd.

CORRUPTION PLANS
U.S. 19 (CORRUPTION PLANS PLANS

BY OTHERS



FDOT ACCESS PERMIT NO. _____
FDOT DRAINAGE PERMIT NO. _____
SWFWMD EXEMPTION NO. _____

ROADWAY IMPROVEMENTS

FOR

U.S. 19 (Commercial Way) and Glen Lakes Blvd

HERNANDO COUNTY, FLORIDA

PREPARED FOR

Glen Lakes Common, LLC

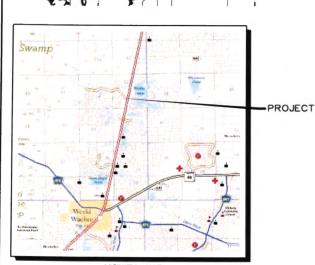
13904 North Boulevard Tampa, Florida 33613

NEW IMPERVIOUS: 600 SF CLEAR ZONE: 36' TRAVEL LANE, 24' AUXILIARY LANE

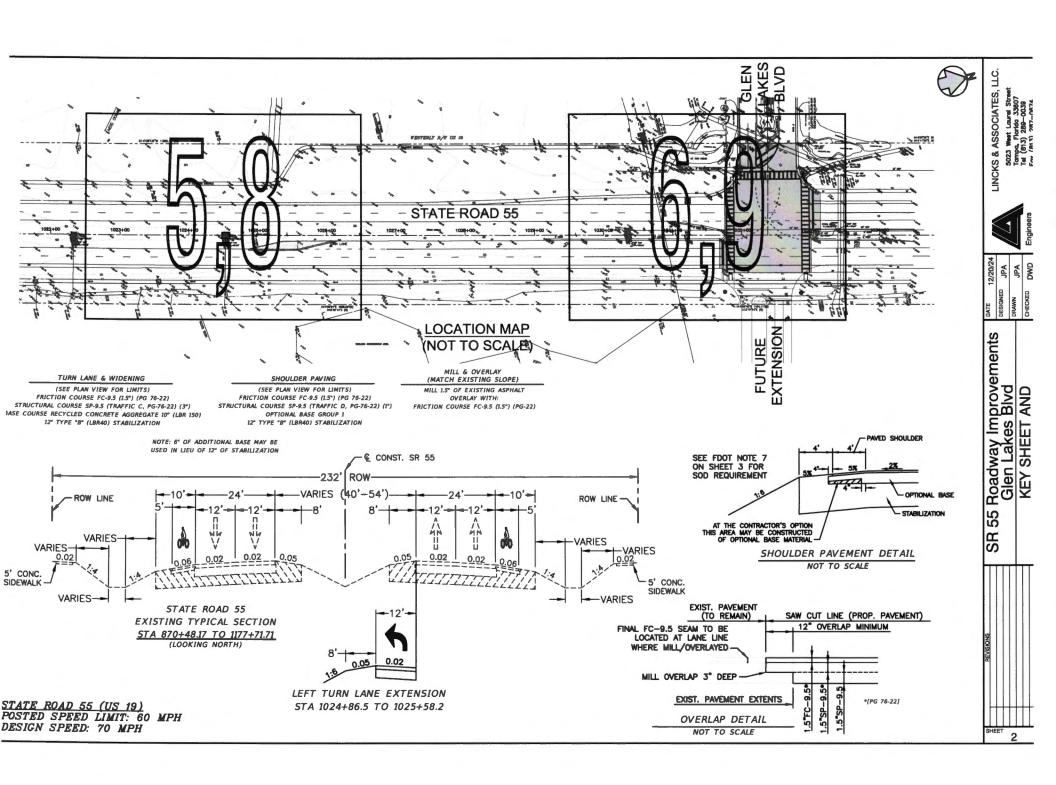
LINCKS & ASSOCIATES, LLC ENGINEERS PLANNERS



	IND	=X	
1 Cover Sheet			
2 Key Sheet &	Typical Sect	ions	
3 General Not	es		
4 Legends & N	viscellaneous	Detail	S
5-6 Roadway			
7-8 Signing ar	nd Pavement	Markini	Plans
9 Guide Sign 1	Worksheet		
DATE:	SIGNED:		JOB NO:
12/20/24 REVISION			SHEET
IL VISION			SHEET



MCINITY MAP
SECT 13 INWISHP 22 S, RNG 17 E
HERNANDO COUNTY, FLORIDA
SR55 SEGMENT 08020000
MP: 10.635



S

NERAL NOTES (LINCKS)

THE CONTRACTOR SHALL VERIFY CONTROL POINTS FOR LINE AND GRADE PRIOR TO NSTRUCTION.

LOCATION, ELEVATION, AND DIMENSIONS OF EXISTING UTILITIES, STRUCTURES, AND HER FEATURES ARE SHOWN IN ACCORDANCE WITH THE BEST INFORMATION AVAILABLE TIME OF THE PREPARATION OF THESE PLANS BUT DO NOT PURPORT TO BE ABSOLUTELY RRECT.

THE CONTRACTOR SHALL VERIFY THE LOCATION, ELEVATION, AND DIMENSIONS OF ALL ISTING UTILITIES, STRUCTURES, AND OTHER FEATURES AFFECTING HIS WORK AND IALL COMPLY WITH ALL STATE, COUNTY, AND LOCAL ORDINANCES AND OBTAIN ANY CESSARY WORK PERMITS THAT MAY BE REQUIRED PRIOR TO CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE COMPLETE COORDINATION OF NSTRUCTION SCHEDULING BETWEEN THE CONTRACTOR AND ALL UTILITY AGENCIES. THE FOR VERTICAL AND HORIZONTAL CLEARANCES TO THE OVERHEAD DISTRIBUTION AND NTRACTOR SHALL ADJUST HIS SCHEDULE TO COINCIDE WITH OTHER REQUIRED ILITIES.

THE ENGINEER RESERVES THE RIGHT TO PERFORM QUALITY ASSURANCE TESTING ON L MATERIALS DELIVERED TO PROJECT.

IN THE EVENT UNSUITABLE MATERIAL IS ENCOUNTERED DURING EXCAVATION IT IS TO REMOVED FROM THE SITE EXCEPT THAT IT MAY BE USED TO FLATTEN SLOPES IN EAS APPROVED BY THE ENGINEERS.

OVERALL CLEANUP SHALL BE ACCOMPLISHED BY THE CONTRACTOR IN ACCORDANCE WITH FRICTION/SURFACE COURSE TO OBLITERATE OBSOLETE PAVEMENT MARKINGS UNTY, CITY AND/OR FOOT STANDARDS OR AS DIRECTED BY THE ENGINEER. ANY AND ALL 2. LANE CLOSURE MAY NEED TO BE ALTERED DEPENDING ON THE AREA THE WORK IS PENSES INCURRED FOR THIS WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR

THE CONTRACTOR SHALL ENDEAVOR TO PROTECT PRIVATE PROPERTY. ANY DAMAGE USED BY THE CONTRACTOR IN THE PERFORMANCE OF HIS WORK SHALL BE CORRECTED THE SATISFACTION OF THE ENGINEER AT THE CONTRACTOR'S EXPENSE. PAYMENT

IALL NOT BE MADE FOR THIS WORK.

ANY DAMAGE TO STATE, COUNTY, OR LOCAL ROADS CAUSED BY THE CONTRACTOR'S UIPMENT AND OPERATORS SHALL BE REPAIRED BY THE CONTRACTOR TO THE TISFACTION OF THE ENGINEER. PAYMENT SHALL NOT BE MADE FOR THIS WORK. ANY U.S.C. AND G.S. MONUMENT WITHIN LIMITS OF CONSTRUCTION SHALL BE OTECTED BY THE CONTRACTOR. IF IN DANGER OF DAMAGE, THE CONTRACTOR SHALL TIFY: FLORIDA'S STATE GEODETIC ADVISOR - RONNIE L. TAYLOR AND RANDY WEGNER C/O 9. DEP, BUREAU OF SURVEY AND MAPPING, MS 105 - 3900 COMMONWEALTH BLVD. LLAHASSEE, FL 32399 TELEPHONE: (850) 245-2606 FAX: (850) 245-2645 TTP://WWW.NGS.NOAA.GOV/ADVISORS/FLORIDA.SHTML)

CONSTRUCTION OF POTABLE WATER AND SANITARY SEWER SYSTEMS ARE TO INCLUDE

NNECTION TO EXIST. FACILITIES AS SHOWN.

COMPACTION FOR PIPE BACKFILL SHALL COMPLY WITH AASHTO AND FDOT. FLORIDA STATUTE REQUIRES A MINIMUM OF 2 DAYS AND A MAXIMUM OF 5 DAYS TICE BEFORE A CONTRACTOR MAY EXCAVATE. THE CONTRACTOR SHALL CALL SUNSHINE 300 432-4770 BEFORE EXCAVATION SHALL COMMENCE.

ALL FILL UP TO THE SPRING LINE OF THE PIPE MUST BE HAND TAMPED. CONTRACTOR SHALL EMAIL TO THE ENGINEER FOR REVIEW, PDF'S OF SHOP DRAWINGS IRE HYDRANTS, VALVES, PIPES, INLETS, MANHOLES, PUMPS, CONTROL PANELS, LIGHT XTURES, PLUMBING FIXTURES, ETC.). THE CONTRACTOR SHALL ALLOW A MINIMUM OF 5 YS FOR REVIEW OF SHOP DRAWINGS. APPROVAL OF SHOP DRAWINGS IS REQUIRED TOR TO INSTALLATION OR CONSTRUCTION. SUBSTITUTION OF SPECIFIED ITEMS, IF

QUESTED, WILL BE EVALUATED BY THE PROJECT ENGINEER.

THE CONTRACTOR SHALL SELECT A LOCAL TESTING LABORATORY FOR ON SITE TESTS. CH AS: CONCRETE STRENGTH, SLUMP, DENSITIES OR LBR OF ROADBEDS, ETC., COPIES ALL TESTS SHALL BE SENT TO THE ENGINEER AND/OR ARCHITECT WITHIN 10 DAYS OF IE TESTS. ALL TESTS WILL BE AT THE EXPENSE OF THE CONTRACTOR. THE NTRACTOR SHALL PERFORM TESTING ON ALL DRIVEWAYS IN ACCORDANCE WITH F.D.O.T. STING REQUIREMENTS. THE FINAL REPORT SHALL INCLUDE ALL TESTING AND A

RTIFICATION THAT ALL TEST/RETEST MEET FOOT STANDARDS. THE CONTRACTOR SHALL CONTACT THE ENGINEER'S OFFICE IMMEDIATELY ON ANY NFLICTS BETWEEN WHAT IS SHOWN HEREIN AND ACTUAL CONDITION DISCOVERED

IRING CONSTRUCTION OF ANY IMPROVEMENTS SHOWN ON THE DRAWINGS. PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL EXPOSE ALL EXISTING UTILITIES BE EXTENDED OR CROSSED. ANY CONFLICTS AND THE CONTRACTOR'S PROPOSED SOLUTIONS SHALL BE BROUGHT TO THE ENGINEER'S ATTENTION FOR APPROVAL.

ALL PRACTICABLE EFFORT SHALL BE TAKEN DURING CONSTRUCTION TO CONTROL AND EVENT EROSION AND TRANSPORT OF SEDIMENT MATERIALS TO INLETS, SURFACE IAINS, WETLANDS AND LAKE AREAS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL STORATION EFFORTS THAT MAY BE REQUIRED.

THE CONTRACTOR IS TO CONTROL ALL FUGITIVE DUST ORIGINATING ON THIS PROJECT

WATERING OR OTHER METHODS AS REQ'D.

THE CONTRACTOR SHALL NOT ENTER UPON OR IN ANY WAY ALTER THE TLAND/CONSERVATION AREAS. ALL WORK IN THE VICINITY OF OPEN WATER AND/OR TLANDS IS TO BE PERFORMED IN COMPLIANCE WITH THE ENVIRONMENTAL REGULATIONS 18. THE FDOT'S STANDARD PLANS FOR MAINTENANCE OF TRAFFIC (MOT) TAKE D/OR PERMITS FOR THE PROJECT. THE CONTRACTOR WILL BE RESPONSIBLE FOR ANY NE RESULTING FROM HIS VIOLATION OF ANY REGULATIONS OR PERMIT CONDITIONS.

IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ESTABLISH IN THE FIELD RIGHT WAY LINES, BASE LINES, BENCH MARKS (ELEV.), CENTER LINES, AND STATIONING AS

QUIRED TO CONSTRUCT THIS PROJECT.

CONTRACTOR SHALL COORDINATE AND REIMBURSE THE APPROPRIATE OVERHEAD ILITY COMPANY FOR REQUIRED SUPPORT OF UTILITY POLES WHERE CONSTRUCTION MAY USE THE POLE TO LOSE ITS SELF SUPPORTING ABILITY.

SPECIAL PIPE FOUNDATIONS, IF REQ'D, SHALL BE FIELD DETERMINED AND THE TYPE OD WILL BE DIRECTED BY THE ENGINEER.

FINISHED GRADE FOR GROUND ELEVATIONS ON DRAWINGS REFER TO GRADE AFTER DDING.

ALL ELEVATIONS ARE NAVD (1988); EQUATION TO NGVD 1929 IS +0.88.

27 ALL EXISTING DRAINAGE STRUCTURES WITHIN THE RIGHT-OF-WAY SHALL REMAIN UNLESS SHOWN ON THE PLANS TO BE REMOVED.

28. THE CONTRACTOR SHALL BE RESPONSIBLE FOR IMPLEMENTING AND MAINTAINING MEASURES TO MITIGATE ADVERSE IMPACT OFF-SITE. THE CONTRACTOR SHALL PROVIDE EROSION AND SEDIMENT CONTROL WHERE SHOWN ON THE PLANS AND WHERE NECESSARY. SILT FENCE SHALL BE PROVIDED PER FDOT EROSION AND SEDIMENT CONTROL DESIGNER & OPERATIONS ON THE STATE HIGHWAY SYSTEM. REVIEWER MANUAL, FIGURE V-2.

29. CONTRACTOR TO RAISE ALL VALVE BOXES AND MANHOLES AS NEEDED DUE TO CONSTRUCTION.

30. THE STORAGE OF ANY MATERIALS OR EQUIPMENT SHALL NOT BE PERMITTED IN AREAS IN OR ADJACENT TO TREE BARRICADES.

31. CONTRACTOR SHALL COMPLY WITH OSHA'S STANDARDS 29 CFR PART 1926. SUBPART CC TRANSMISSION POWER LINES.

. ALL TRAFFIC STRIPES AND MARKINGS ARE TO BE LEAD-FREE, NON-SOLVENT BASED THERMOPLASTIC. THE PERMITTEE SHALL FURNISH THE DEPARTMENT WITH THE MANUFACTURER'S CERTIFICATION THAT THE THERMOPLASTIC IS "LEAD FREE." REMOVAL OF EXISTING STRIPING SHALL BE DONE BY MILLING AND RESURFACING OF THE

TAKING PLACE

3. CALL FDOT @ 813-612-3214 TO SCHEDULE A PRE-CONSTRUCTION MEETING. PER FAC RULE CHAPTER 14-96.007(5)(C)6, BEFORE CONSTRUCTION BEGINS, THE APPLICANT OR THE APPLICANTS CONTRACTOR SHALL DELIVER TO THE DEPARTMENT PROOF OF LIABILITY INSURANCE IN THE AMOUNT OF \$500,000.

5. A COPY OF THE APPROVED PERMIT AND DRAWINGS MUST BE ON THE JOB SITE FOR WORK TO BEGIN IN THE FDOT RIGHT OF WAY.

6. ANY SIDEWALK DISTURBED WILL BE REPLACED, BY SECTION, TO FDOT SPECIFICATIONS. ALL PORTIONS OF FDOT RIGHT OF WAY DISTURBED MUST BE SODDED.

NEW SIDEWALK PERMITTED TO CITY OR COUNTY ONLY AND SHALL INCORPORATE HANDICAP ACCESSIBLE RAMPS WHERE SIDEWALK INTERSECTS ROADS AND STREETS.

IN ACCORDANCE WITH FLORIDA STATUTE 335,1825 (2), "THE PERMITTEE, HOWEVER, SHALL BEAR THE COST OF ALTERATION OF ANY CONNECTION WHICH IS REQUIRED BY THE DEPARTMENT DUE TO INCREASED OR ALTERED TRAFFIC FLOWS GENERATED BY CHANGES IN THE FACILITIES OR NATURE OF BUSINESS CONDUCTED AT THE LOCATION SPECIFIED IN THE PERMIT, IF THE DEPARTMENT ESTABLISHES THE NEED FOR SUCH ALTERATION."

ALL CONSTRUCTION AND/OR MAINTENANCE IN THE FDOT R/W SHALL CONFORM TO THE FEDERAL MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD), THE DEPARTMENT'S ROADWAY AND TRAFFIC DESIGN STANDARDS AND THE STANDARD SPECIFICATIONS FOR ROAD AND RRIDGE CONSTRUCTION

11. IF CONSTRUCTION, RECONSTRUCTION, REPAIR OR MAINTENANCE ACTIVITY NECESSITATES THE CLOSING OF ONE OR MORE TRAVEL LANES OF ANY ROAD ON THE STATE PRIMARY, COUNTY ROAD OR CITY STREET SYSTEM, FOR A PERIOD OF TIME EXCEEDING TWO HOURS, THE PARTY PERFORMING SUCH WORK WILL BE RESPONSIBLE TO GIVE NOTICE TO THE APPROPRIATE LAW ENFORCEMENT AGENCY WHICH HAS JURISDICTION WHERE SUCH ROAD IS LOCATED PRIOR TO COMMENCING WORK ON THIS PROJECT. FLORIDA STATUTE 335.15 AND 336.07

12. OPEN CUTTING OF ROADWAY FOR INSTALLATION OF UTILITY OR DRAINAGE FACILITIES WILL NOT BE ALLOWED WITHOUT PRIOR APPROVAL FROM FDOT. ANY PROPOSED OPEN CUTS MUST BE CLEARLY NOTED ON THE PLANS AND A SEPARATE LETTER OF APPROVAL MUST BE OBTAINED.

SIDEWALK SHALL BE CONSTRUCTED PER FDOT S.P. 522-002 AND S.P. 522-001. DETECTABLE WARNING STRIPS, A.K.A. TRUNCATED DOMES, FOR DISTRICT SEVEN CONSTRUCTION AND MAINTENANCE SHOULD BE EITHER INSET CERAMIC TILES OR THERMOPIASTIC DETECTARIE WARNING STRIPS THESE WARNING SURFACES SHALL ONLY RE PROVIDED BY THE VENDORS/MANUFACTURERS LISTED IN THE FOOT'S APL OR AS APPROVED BY THE ENGINEER.

14. DRIVEWAYS ARE TO BE CONSTRUCTED PER FDOT S.P. 330-001. THOUGH S.P. 330-001 ALLOWS A 2% SLOPE ACROSS SIDEWALKS, CURRENT ADA REQUIREMENTS ARE THAT 2% IS THE MAXIMUM ALLOWABLE SLOPE AS CONSTRUCTED. SIDEWALK SLOPES WITHIN FOOT RIGHT OF WAY SHALL BE AT 1.5% TO ALLOW CONSTRUCTION VARIANCES.

15. TRAFFIC SIGNALS ARE TO REMAIN IN ACTUATION DURING CONSTRUCTION. 16. ALL PROPOSED TRAFFIC SIGNS AND PAVEMENT MARKINGS SHALL BE IN CONFORMANCE WITH AND CONSTRUCTED TO THE FOOT STANDARD PLANS SERIES 700. THE PROPERTY OWNER MUST MAINTAIN THE TRAFFIC SIGNS AND MARKINGS FOR THE DRIVEWAY. 17. SILT FENCE FOR EROSION CONTROL IN CONFORMANCE WITH FDOT EROSION AND SEDIMENT CONTROL DESIGNER & REVIEWER MANUAL, FIGURE V-2 IS REQUIRED FOR ALL AREAS OF CONSTRUCTION WITHIN FOOT R/W: PERPENDICULAR TO AND PARALLEL WITH THE STATE ROAD.

PRECEDENCE FOR WORK WITHIN FOOT RIGHT OF WAY, IF THERE ARE CONFLICTING NOTES OR DETAILS BETWEEN MUNICIPALITY AND FOOT

LANE CLOSURE RESTRICTIONS SHALL APPLY EQUALLY TO BOTH MOTOR VEHICLE AND 19. BICYCLE TRAFFIC

ALL AREAS IN THE FDOT RIGHT OF WAY DISTURBED DURING CONSTRUCTION SHALL BE RESTORED, COMPACTED, SODDED AND WATERED IN CONFORMANCE WITH THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 575 AND 981 AND STANDARD PLAN 570-010

21. RESILIENT CONNECTORS SHALL BE USED ON ALL STORMWATER PIPE TO STRUCTURE CONNECTIONS PER 942-3 STANDARD SPECIFICATIONS FOR ROAD & BRIDGE CONSTRUCTION JULY 2019

22. EROSION CONTROL DEVICES SHALL BE PER STATE OF FLORIDA EROSION AND SEDIMENT CONTROL DESIGNER AND REVIEWER MANUAL LATEST EDITION.

FDOT MAINTENANCE GENERAL NOTES

ALL WORK PERFORMED IN THE DEPARTMENT'S RIGHT OF WAY SHALL BE DONE IN ACCORDANCE WITH THE MOST CURRENT EDITIONS (AT TIME OF APPROVAL) OF:

F.D.O.T. STANDARD PLANS FOR ROAD AND BRIDGE CONSTRUCTION.

F.D.O.T. DESIGN STANDARDS FOR DESIGN, CONSTRUCTION, MAINTENANCE AND UTILITY

F.D.O.T. DESIGN MANUAL, PART 2.

F.D.O.T. FLEXIBLE PAVEMENT DESIGN MANUAL FOR NEW CONSTRUCTION AND PAVEMENT REHABILITATION.

ALL AREAS IN THE FOOT RIGHT OF WAY DISTURBED DURING CONSTRUCTION SHALL BE RESTORED, COMPACTED, SODDED AND WATERED IN CONFORMANCE WITH THE FDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION, SECTION 575 AND 981 AND STANDARD PLAN 570-010.

DAYTIME WORK ONLY, BETWEEN THE HOURS OF 9AM & 3:30PM UNLESS OTHERWISE APPROVED BY F.D.O.T. FOR NIGHTTIME OR EXTENDED HOURS. THE FDOT ENGINEER MUST APPROVE NIGHT WORK AND LANE CLOSURES AT LEAST 48 HOURS IN ADVANCE. THE APPLICANT MUST NOTIFY THE FDOT LOCAL MAINTENANCE OFFICE 48 HOURS IN ADVANCE OF STARTING ANY PROPOSED WORK. CALL: BROOKSVILLE MAINTENANCE, JAMES MEYER @ 352-848-2600.

ALL TRAFFIC STRIPES AND MARKINGS ARE TO BE LEAD-FREE, NON-SOLVENT BASED THERMOPLASTIC. THE PERMITTEE SHALL FURNISH THE DEPARTMENT WITH THE MANUFACTURER'S CERTIFICATION THAT THE THERMOPLASTIC IS "LEAD FREE." REMOVAL OF EXISTING STRIPING SHALL BE DONE BY MILLING AND RESURFACING OF THE FRICTION/SURFACE COURSE TO OBLITERATE OBSOLETE PAVEMENT MARKINGS.

ALL PROPOSED TRAFFIC SIGNS AND PAVEMENT MARKINGS SHALL RE IN CONFORMANCE WITH AND CONSTRUCTED TO THE EDOT STANDARD PLANS, 700 SERIES. THE PROPERTY OWNER MUST MAINTAIN THE TRAFFIC SIGNS AND MARKINGS FOR THE DRIVEWAY.

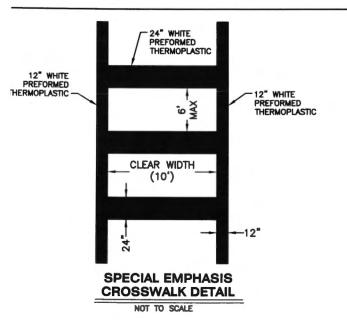
FOR WORK WITHIN FOOT RIGHT-OF-WAY, THE CONTRACTOR SHALL PROVIDE A TEMPORARY TRAFFIC CONTROL PLAN PREPARED UNDER THE DIRECTION OF, AND SIGNED AND SEALED BY A LICENSED FLORIDA PROFESSIONAL ENGINEER WHO IS EXPERIENCED IN PREPARING TRAFFIC CONTROL PLANS AND WHO IS CERTIFIED PER FDOT PROCEDURE TOPIC NO. 625-010-010. THE FDOT REQUIRES DOCUMENTATION FOR SUCCESSFUL COMPLETION OF AN APPROVED WORK ZONE TRAFFIC CONTROL TRAINING COURSE FOR THE AGENCY, UTILITY OR CONTRACTOR EMPLOYEE(S) DESIGNING, INSTALLING AND/OR MAINTAINING THE APPROVED MAINTENANCE OF TRAFFIC PLAN. DOCUMENTATION IS TO BE FURNISHED TO THE DEPARTMENT AT THE PRE-CONSTRUCTION MEETING OR BEFORE OCCUPYING STATE RIGHT

MAINTENANCE OF TRAFFIC PLAN FOR WORK ZONES SHALL BE IN CONFORMANCE WITH THE REQUIREMENTS OF FDOT STANDARD PLAN 102-600 AND IMPLEMENTED PER THE SPECIFIC REQUIREMENTS OF THE CURRENT FOOT STANDARD PLAN 102 SERIES INCLUDED WITH THESE PLANS

ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL BE DESIGNED AND INSTALLED TO MEET THE EXISTING POSTED SPEEDS

MAINTENANCE OF TRAFFIC PLAN MUST INCLUDE FOOT STANDARD PLAN 102-660. PEDESTRIAN CONTROL FOR CLOSURE OF SIDEWALKS.

ALL MATERIALS AND PRODUCTS INSTALLED IN THE FDOT RIGHT-OF-WAY (INCLUDING TACTILE SURFACES) SHALL BE IN CONFORMANCE WITH THE FDOT'S APPROVED PRODUCTS LIST, AND THE MATERIALS SOURCE LISTING (TIMBER AND PIPE, ETC).



UTILITY LEGEND

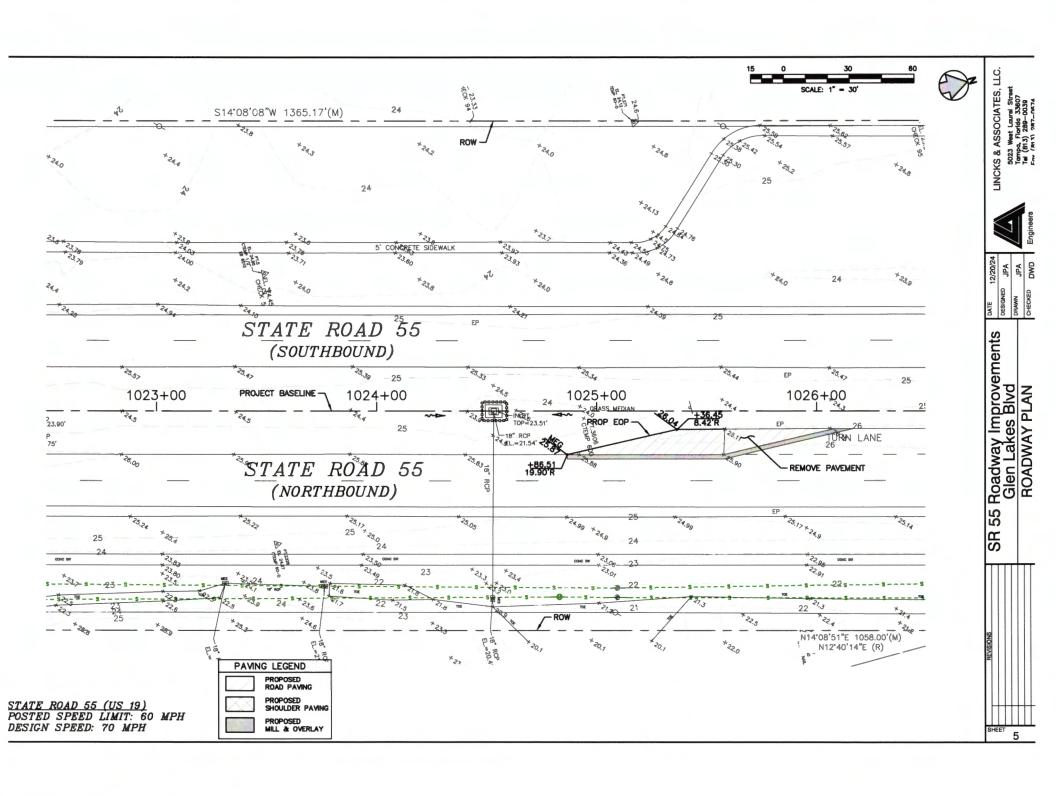
EXISTING	PROPOSED		
w		WATERMAIN	
NPW		RECLAIMED WATERMAIN	
BFO		COMMUNICATIONS	
s		FORCEMAIN	
G		GAS	
BT		BURIED TELEPHONE	
BE		BURIED ELECTRIC	
SEE PLANS PROFIL	FS & CROSS SI	FCTIONS FOR SIZES	

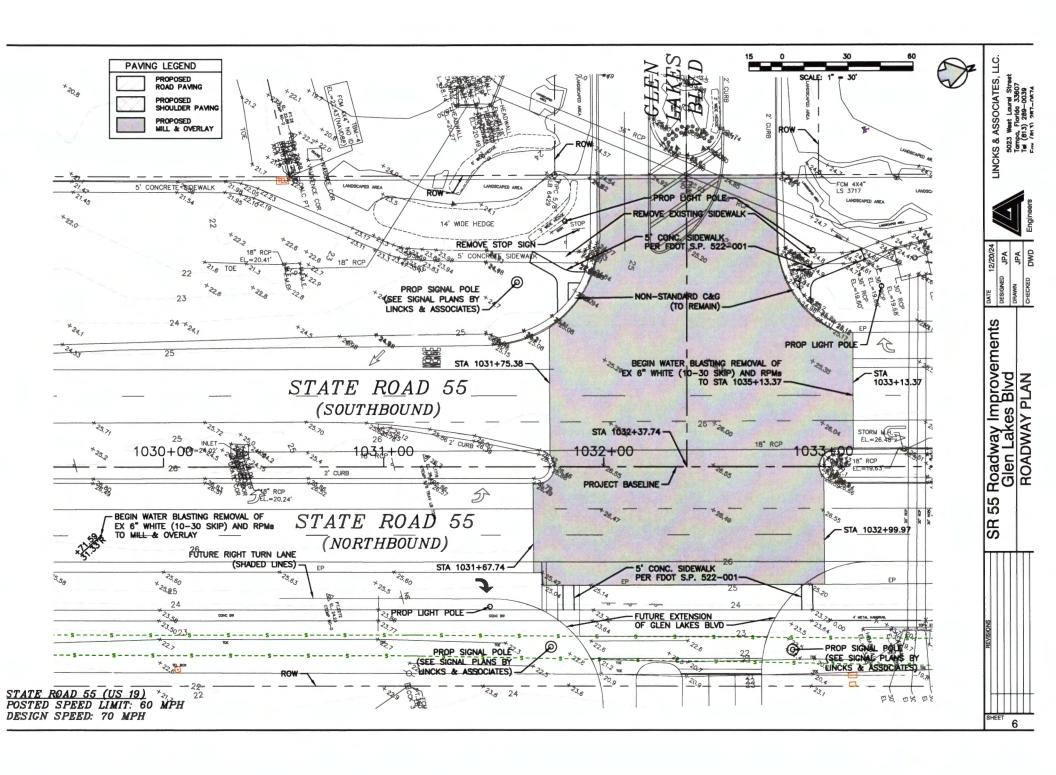


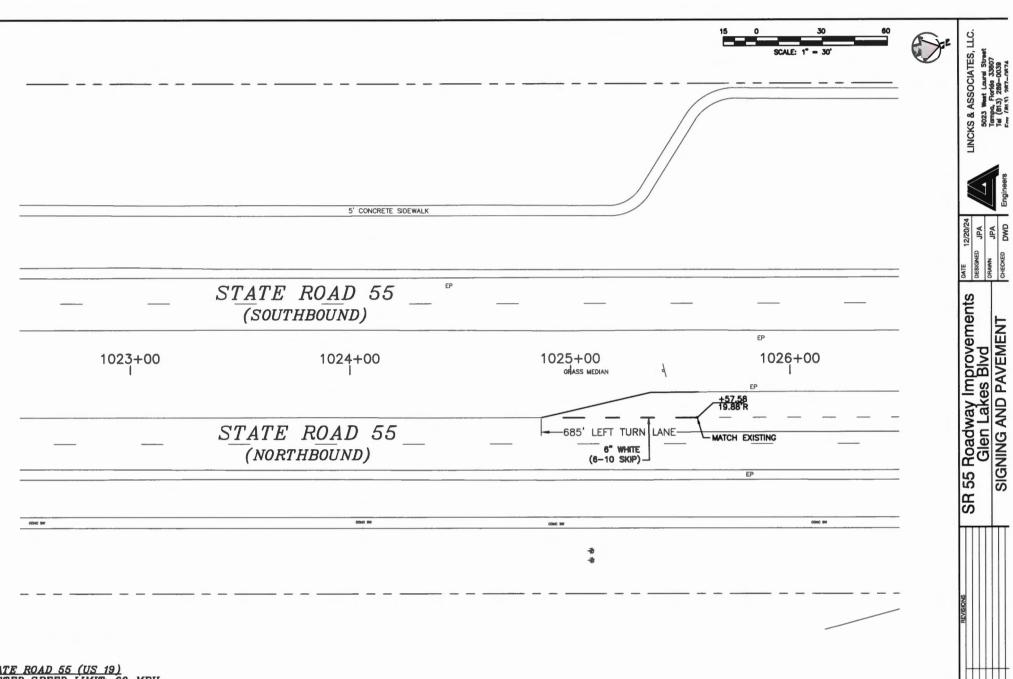
500	Edna of Downwood
BOW	Edge of Pavement
TOB	Right of Way Top of Bank
706	Toe of Slope
Conc	Concrete
Elev	Elevation
7.E	Invert Elevation
	Reinforced Concrete Pipe
CIP	Cast Iron Pipe
PVC	Poly Vinyl Chloride
CMP	Corrugated Metal Pipe
DIP	Ductile Iron Pipe
SHWL	Seasonal High Water Level
MFG	Match Existing Grade
D.E	Drainage Easement Normal Crown Finished Floor Elevation
NC	Normal Crown
FFE	Finished Floor Elevation
SSMH	Sanitary Sewer Manhole
£	Centerline of Right-of-Way
P	Property Line
<u>co</u>	Clean Out
0	Olddii Out

ROADWAY LEGEND

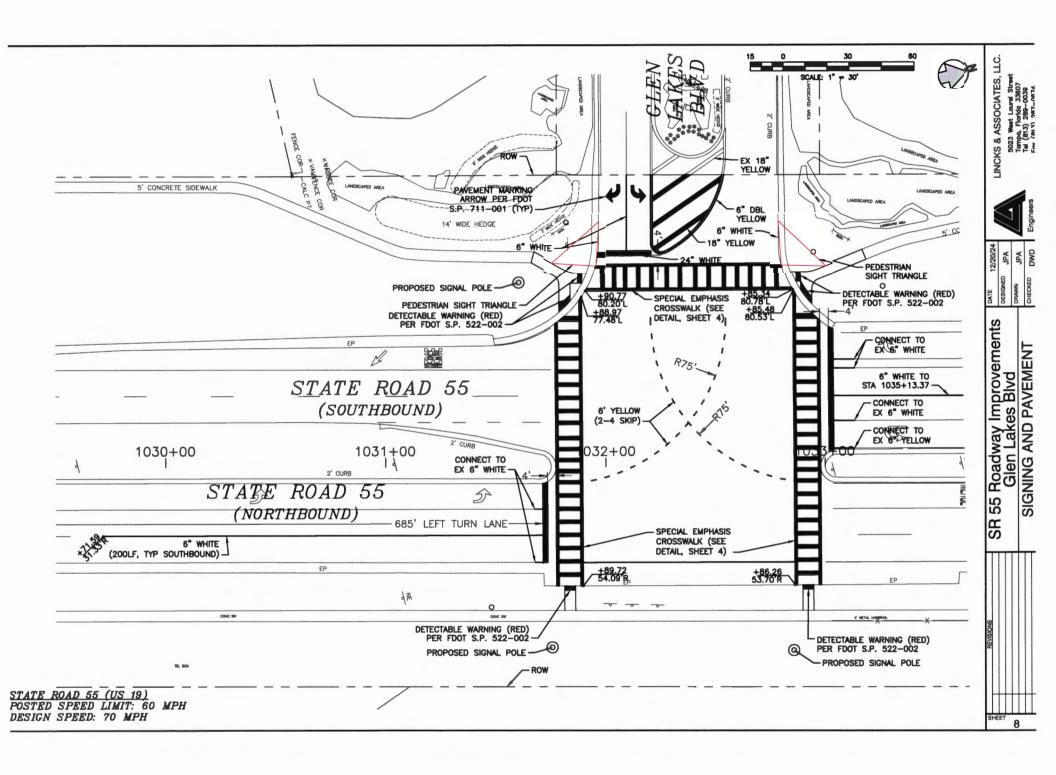
EXISTING	PROPOSED	
		STORM DRAINAGE STRUCTURE
× 15.00	23.33	ELEVATION
15	15	CONTOUR
~	~	DIRECTION OF SURFACE FLOW
	0.0.0.0.0.0.0.0.0	STAKED EROSION CONTROL
		RIPRAP 3-4" DIA. 6'x6'x6" DEEP ON FABRIC (FDOT SPEC 530)







STATE ROAD 55 (US 19) POSTED SPEED LIMIT: 60 MPH DESIGN SPEED: 70 MPH



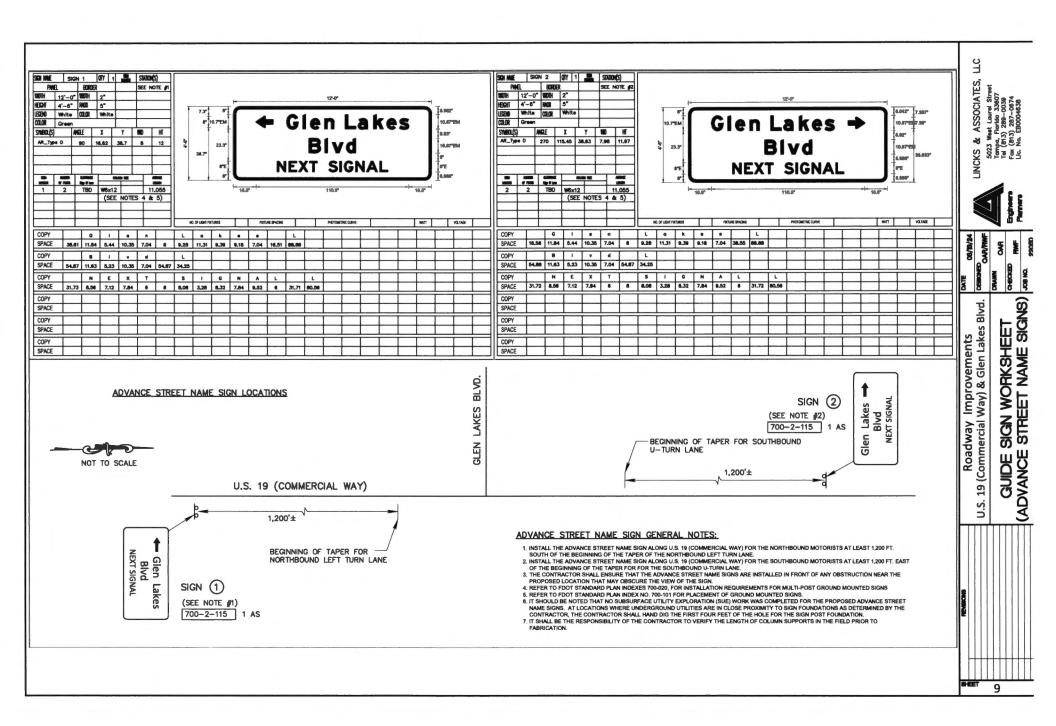


Exhibit "D"

Estimated Project Costs

ENGINEER'S CONSTRUCTION COST ESTIMATE

SIGNALIZATION PROJECT

U.S. 19 @ GLEN LAKES BOULEVARD HERNANDO COUNTY, FLORIDA

Estimated By: Company: Date: Project No.:	Robert W. Fulp, PE (Engineer-o Lincks & Associates, Inc. 7-Feb-25 22050 / F4	-Record)
Roadway	\$ 171,01	
Signing / Pavement Markings Signalization & Lighting	\$ 15,000 \$ 796,029	
Sub-Total	\$ 982,03	7
Contingency Items (10%)	\$ 98,20	1 (3)
Maintenance of Traffic (5%)	\$ 49,10	2
Mobilization (5%)	\$ 49,10	2
Bonding (3%)	\$ 29,46	L,
MOT Plans	\$ 3,500	(2)
Construction Surveying	\$ 5,000	(2)
Permits	\$ 2,000	(2)
Material Testing	\$ 5,000	(2)
As-Builts	\$ 3,500	(2)
Sub-Total	\$ 1,226,900	5
Independent Contractors Fee (8%)	\$ 98,15	2
Grand Total	\$ 1,325,05	3

Notes:

- (1) The estimate was based on FDOT's current 12-month (12/1/23 11/31/24) moving average unit costs for Area 7, which includes Hernando County. See attached plans.
- (2) This is an estimated value based on the size of the project.
- (3) Due to project unknowns, such are unexpected utility relocations, etc., a continency of 10% was assumed.

Exhibit "E"

Transportation Analysis



February 10, 2025

Cliff Manuel Coastal Engineering 966 Candlelight Boulevard Brooksville, FL 34601

Re: Project Name: US 19/Glen Lakes Boulevard

Lincks Project No: 22050

Dear Mr. Manuel,

The purpose of this letter is to provide the proportionate share cost for the signalization of the intersection of US 19 and Glen Lakes Boulevard. The following methodology was utilized to calculate the proportionate share:

1. The following formula was utilized to determine the proportionate share of the required improvements.

Proportionate Share – (Project Traffic/LOS D Capacity) x (Roadway Cost)

- The cost of the improvements are based on the cost estimate dated 1/27/25 See Appendix.
- 3. The project traffic was obtained from the Access Management Analysis conducted for the project dated April, 2024.
- The proportionate share percentage was calculated as shown in Table 1-A included in the appendix of this letter.

Table 1 provides the proportionate share cost which was calculated to be \$349,020.

It should be noted that the proposed development located north of Glen Lakes Boulevard is not anticipated to add much if any additional trips to the subject intersection for the following reasons:

1. The project will have access to the median opening at the Publix Shopping Center.

TABLE 1

INTERSECTION PROPORTIONATE SHARE DETERMINATION

Intersection	Improvement	Improvement Cost (1)	Percentage of LOS D Capacity Consumed by Project Traffic (2)	Proportionate <u>Share</u>
US 19 and Glen Lakes Blvd	Signal	\$1,325,058	26.34%	\$349,020

⁽¹⁾ Source: FDOT Roadway Cost Per Centerline Mile, Revised August, 2023.

^{(2) %} LOS D Consumed-See Table A-1 in the Appendix.

Mr. Cliff Manuel February 10, 2025 Page 3

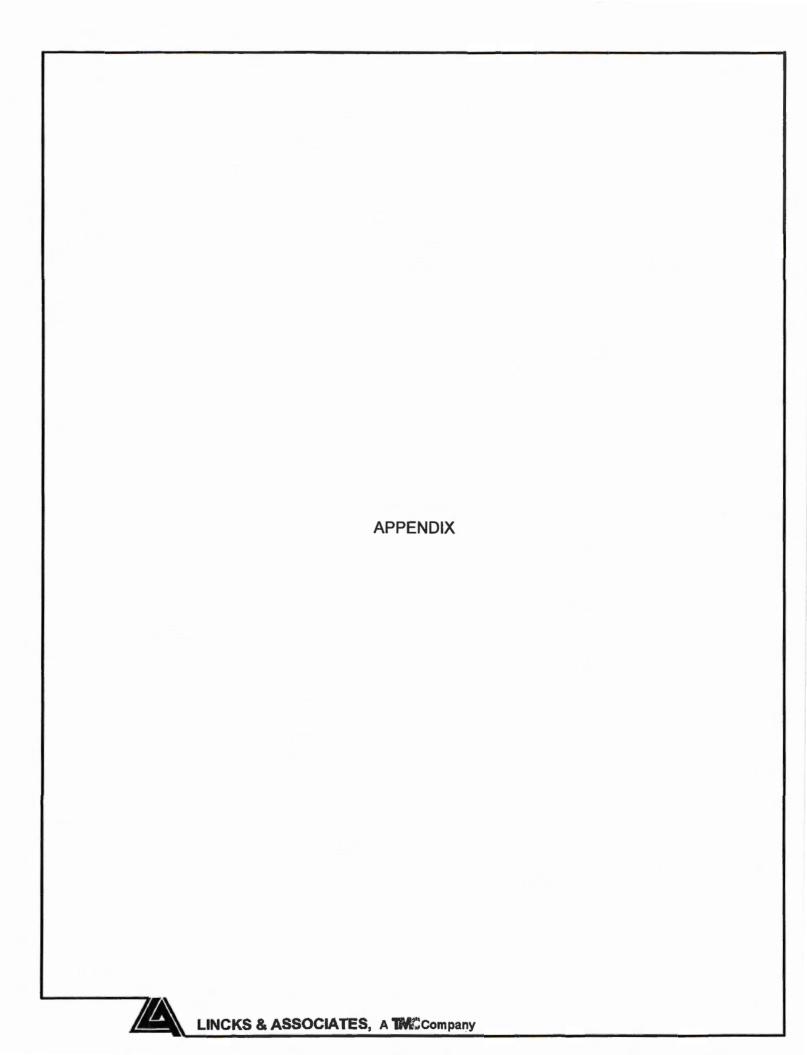
- 2. The project would also have access to the intersection of US 19 and Bourassa Boulevard that is to be signalized.
- 3. Finally, if the project traffic for the proposed development goes to the Publix Shopping Center then to Glen Lakes Boulevard it would be included in the Publix Shopping Center traffic.

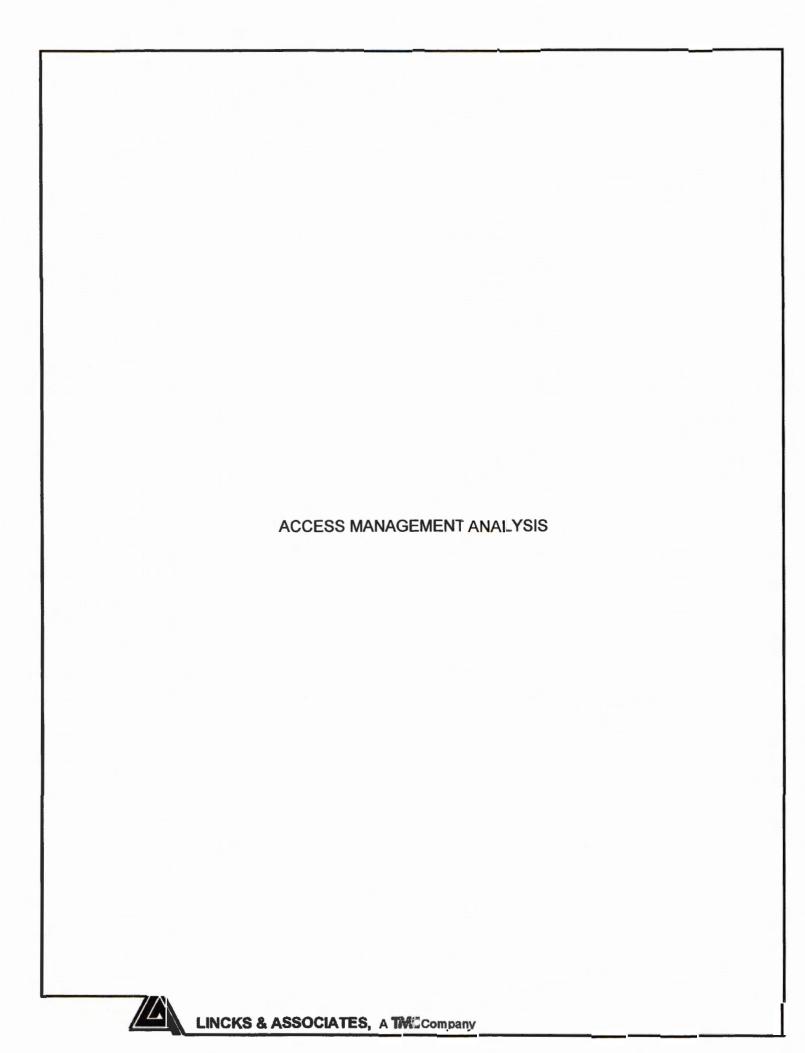
If you have any questions, please do not hesitate to contact me at shenry@lincks.com or 813-559-9589.

LINCKS & \$5500 INTES, LLC

A TMC Company

Steven Henry, PE President





ACCESS MANAGEMENT ANALYSIS

GLEN LAKES COMMONS

Prepared For

GLEN LAKES COMMONS, LLC.

Prepared By



ACCESS MANAGEMENT ANALYSIS

GLEN LAKES COMMONS

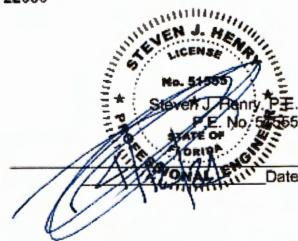
Prepared For GLEN LAKES COMMONS, LLC.

Prepared By

LINCKS & ASSOCIATES, INC. 5023 West Laurel Street Tampa, Florida 33607 813-289-0039 State of Florida Authorization No. EB0004638

> Revised April, 2024 November, 2023

Project No. 22050





LINCKS & ASSOCIATES, INC.

INTRODUCTION

The purpose of this report is to provide an Access Management Analysis for the proposed development located south of Glen Lakes Boulevard and west of US 19 in Hernando County, Florida, as shown in Figure 1. The subject property is proposed to be developed for the following land uses:

- Assisted Living 210 Beds
- Hotel 80 Rooms
- Mini-Warehouse 100,000 Square Feet
- Fast Food Restaurant w/Drive Thru 3,000 Square Feet
- Fast Food Restaurant w/Drive Thru 4,500 Square Feet
- Convenience Store/Gas Station 5,200 Square Feet / 12 Fueling Positions
- Fast Food Restaurant w/Drive Thru 3,500 Square Feet
- Medical-Dental Office Building 20,000 Square Feet

The access for the project is proposed to be as follows:

- One (1) full access to Glen Lakes Boulevard to align with Outer Banks Road
- One (1) right-in/right-out access to US 19

This report will evaluate the access to serve the project. A copy of the site plan is included in the Appendix of this report.

ESTIMATED DAILY PROJECT TRAFFIC

The trip rates utilized in this report were obtained from the latest computerized version of



TABLE 1
ESTIMATED DAILY PROJECT TRIP ENDS

Land Use	ITE LUC	Size	Daily Trip Ends (1)	Passerby Capture (2)	New Daily Trip Ends
Assisted Living	254	210 Beds	546	0	546
Hotel	310	80 Rooms	639	0	639
Mini-Warehouse	151	100,000 SF	145	0	145
Fast Food Restaurant w/Drive Thru	934	3,000 SF	1,402	382	1,020
Fast Food Restaurant w/Drive Thru	934	4,500 SF	2,104	573	1,531
Convenience Store/Gas Station	945	5,200 SF	3,642	1,487	2,155
Fast Food Restaurant w/Drive Thru	934	3,500 SF	1,636	445	1,191
Medical-Dental Office Building	720	20,000 SF	<u>720</u>	<u>0</u>	<u>720</u>
		Total	10,834	2,887	7,947

- (1) Source ITE <u>Trip Generation Manual</u>, 11th Edition, 2021.
- (2) Source ITE <u>Trip Generation Manual</u>, 11th Edition, 2021.
 - · Passerby Percentage:

Fast Food Restaurant w/Drive Thru - 50%

 $(3,000 SF) 1,402 \times 0.50 = 701$

 $(4,500 SF) 2,104 \times 0.50 = 1,052$

 $(3,500 \text{ SF}) 1,636 \times 0.50 = 818$

Convenience Store/Gas Station - 75%

 $3,642 \times 0.75 = 2,732$

- Passerby should not exceed 10% of the adjacent street traffic.
 - $28,867(a) \times 0.10 = 2,887 < 5,303$
 - (a) Based on PM peak hour background traffic at the intersection of US 19 and Glen Lakes Boulevard and K = 0.09.
- · Adjust passerby trips to 2,887 trip ends.

4

TABLE 2
ESTIMATED AM PEAK HOUR PROJECT TRIP ENDS

	ΠE			Peak I		_	assert apture			M Pea	ık Hour Is
Land Use	LUC	Size	<u>In</u>	Out	Total	<u>In</u>	Out	Total	<u>In</u>	Out	Total
Assisted Living	254	210 Beds	23	15	38	0	0	0	23	15	38
Hotel	310	80 Rooms	18	15	33	0	0	0	18	15	33
Mini-Warehouse	151	100,000 SF	5	4	9	0	0	0	5	4	9
Fast Food Restaurant w/Drive Thru	934	3,000 SF	68	66	134	14	14	28	54	52	106
Fast Food Restaurant w/Drive Thru	934	4,500 SF	103	98	201	22	20	42	81	78	159
Convenience Store/Gas Station	945	12 FP	162	162	324	50	50	100	112	112	224
Fast Food Restaurant w/Drive Thru	934	3,500 SF	80	76	156	17	16	33	63	60	123
Medical-Dental Office Building	720	20,000 SF	<u>45</u>	<u>12</u>	<u>57</u>	0	<u>0</u>	0	<u>45</u>	12	<u>57</u>
		Total	504	448	952	103	100	203	401	348	749

⁽¹⁾ Source - ITE Trip Generation Manual, 11th Edition, 2021.

Passerby Percentage:

Fast Food Restaurant w/Drive Thru - 50%

(3,000 SF)

 $\ln : 68 \times 0.50 = 34$

Out: $66 \times 0.50 = 33$

(4,500 SF)

 $\ln 103 \times 0.50 = 52$

Out: $98 \times 0.50 = 49$

(3,500 SF)

 $\ln: 80 \times 0.50 = 40$

Out: $76 \times 0.50 = 38$

Convenience Store/Gas Station - 76%

ln: 162 x 0.76 = 123

Out: 162 x 0.76 = 123

• Passerby should not exceed 10% of the AM peak hour Background traffic.

2,031(a) x 0.10 = 203 < 492

(a) AM peak hour background traffic at the intersection of US 19 and Glen Lakes Boulevard.

· Adjust passerby trips to 203.

⁽²⁾ Source - ITE Trip Generation Manual, 11th Edition, 2021.



	ΠE			Peak I			assert apture			M Pea	k Hour Is
Land Use	LUC	Size	<u>In</u>	Out	Total	<u>ln</u>	Out	Total	<u>in</u>	<u>Out</u>	Total
Assisted Living	254	210 Beds	20	30	50	0	0	0	20	30	50
Hotel	310	80 Rooms	16	15	31	0	0	0	16	15	31
Mini-Warehouse	151	100,000 SF	7	8	15	0	0	0	7	8	15
Fast Food Restaurant w/Drive Thru	934	3,000 SF	51	48	99	18	16	34	33	32	65
Fast Food Restaurant w/Drive Thru	934	4,500 SF	77	72	149	26	25	51	51	47	98
Convenience Store/Gas Station	945	5,200 SF	142	142	284	67	67	134	75	75	150
Fast Food Restaurant w/Drive Thru	934	3,500 SF	60	56	116	21	20	41	39	36	75
Medical-Dental Office Building	720	20,000 SF	<u>23</u>	<u>55</u>	<u>78</u>	<u>0</u>	<u>0</u>	0	23	<u>55</u>	<u>78</u>
		Total	396	426	822	132	128	260	264	298	562

- (1) Source ITE Trip Generation Manual, 11th Edition, 2021.
- (2) Source ITE Trip Generation Manual, 11th Edition, 2021.
 - Passerby Percentage:

Fast Food Restaurant w/Drive Thru - 55%

(3,000 SF)

In: $51 \times 0.55 = 28$

Out: $48 \times 0.55 = 26$

(4,500 SF)

 $h: 77 \times 0.55 = 42$

Out: 72 x 0.55 = 40

(3,500 SF)

 $\ln: 60 \times 0.55 = 33$

Out: $56 \times 0.55 = 31$

Convenience Store/Gas Station - 75%

In: $142 \times 0.75 = 106$

Out: 142 x 0.75 = 106

- Passerby should not exceed 10% of the PM peak hour Background traffic.
 - $2,598(a) \times 0.10 = 260 < 412$
 - (a) PM peak hour background traffic at the intersection of US 19 and Glen Lakes Boulevard.
- · Adjust passerby trips to 260.

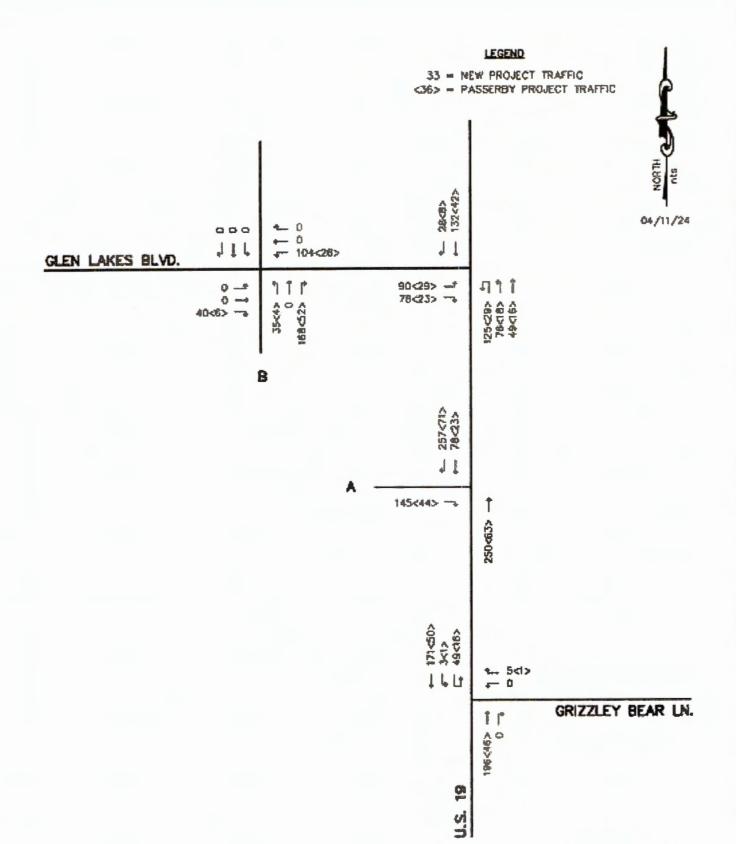


FIGURE 2 AM PEAK HOUR PROJECT TRAFFIC



LEGEND

13.675 59.630 42.6224

174<B9>

33 = NEW PROJECT TRAFFIC <36> = PASSERBY PROJECT TRAFFIC



	900	%— 0 ← 0 ← 80<42>	← 21<0.2> ← B5<48>
GLEN LAKES BLVD.	0 -3 0 -3 26<4> -3	30<7> 4	77<40> 9 67<28>>

B

> - 147675 - 4625 - 42625

> > U.S.

GRIZZLEY BEAR LN.

FIGURE 3
PM PEAK HOUR
PROJECT TRAFFIC



		(23) =	LEGEND EXISTING TRAFFIC NEW PROJECT TRAFFIC PASSERBY PROJECT TRAFFIC
GLEN LAKES BLVD.	€ 5 ← 70 ← (104)<26>	← 15(28)<6> ← 918(132)<42> ⊏ 12	## ## ## ## ## ## ## ## ## ## ## ## ##
19 →9 98 → (40)<6> →	(35)<4> (25) 0 + 0 (168) d2> -	30(90)<29> =f 71(78)<23> =;	(125)-29> ± 57(76)-48> ± 525(49)-46> ± 525(49)-46>
	B	(145)<445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <450) <445) <445) <445) <445) <445) <445) <445) <445) <445) <445) <	†
		→ 848(171)<50> ← 35(3)<1> □ (49)<16>	47(5)<0>>
			GRIZZLEY BEAR LN.

FIGURE 7
AM PEAK HOUR
BACKGROUND PLUS
PROJECT TRAFFIC



			13 = (23) = <36> =		FFIC TRAFFIC ROJECT TRAFFIC
GLEN LAKES BLVD.	1 38 1 0	t= 4 → 119 ← (80)<42>	± 37(21)<12> ← 1085(85)<49> (⊊ 33		E 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2 2
	37 → 82 → (26)<4> ¬	0.30)<5> (30)<5> (144)<660 (144)	20(77)<40>* 54(67)<28>>	4(73)<37> \$2 83(59)<30> ~ 1150(42)<22> ~	
	1	В	- (158)<86>		
		A	(124)<53>	1237(174)<89>	
			1095(145) c57> 47(4) c2> (42) c22>	2 54(4) €	>
			S. 19	t t t t t t t t t t t t t t t t t t t	GRIZZLEY BEAR LN.

FIGURE 8
PM PEAK HOUR
BACKGROUND PLUS
PROJECT TRAFFIC



	1	*	1	1	L	1	1
Lane Group	EBL	EBR	NBL	NBT	SBU	SBT	SBR
Lane Configurations	*5	75	ሻ	^	Ð	ተተ	19
Traffic Volume (vph)	149	172	305	890	12	1092	51
Future Volume (vph)	149	172	305	890	12	1092	51
Peak Hour Factor	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Heavy Vehicles (%)	2%	8%	4%	6%	2%	5%	8%
Shared Lane Traffic (%)							
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	pm+ov
Protected Phases	4	5	5	2		6	4
Permitted Phases		4	2		6		6
Detector Phase	4	5	5	2	6	6	4
Switch Phase							
Minimum Initial (s)	5.0	5.0	5.0	7.0	7.0	7.0	5.0
Minimum Split (s)	12.5	10.5	10.5	14.5	14.5	14.5	12.5
Total Split (s)	30.0	40.0	40.0	110.0	70.0	70.0	30.0
Total Split (%)	21.4%	28.6%	28.6%	78.6%	50.0%	50.0%	21.4%
Yellow Time (s)	5.5	3.5	3.5	5.5	5.5	5.5	5.5
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Lost Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Lead/Lag		Lead	Lead		Lag	Lag	
Lead-Lag Optimize?		Yes	Yes		Yes	Yes	
Recall Mode	None	C-Max	C-Max	C-Max	None	None	None
Act Effct Green (s)	18.1	64.5	108.9	106.9	62.5	62.5	88.1
Actuated g/C Ratio	0.13	0.46	0.78	0.76	0.45	0.45	0.63
v/c Ratio	0.74	0.28	0.62	0.39	0.06	0.81	0.06
Control Delay	77.6	21.4	32.6	6.4	23.1	38.7	2.2
Queue Delay	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Total Delay	77.6	21.4	32.6	6.4	23.1	38.7	2.2
LOS	E	С	C	A	С	D	Α
Approach Delay	47.5			13.1		37.0	
Approach LOS	D			В		D	
Intersection Summary	Nation 18 A				See A	SW HER	
	ALCOHOL SE			No. 11 Acres		140040192	
Cycle Length: 140 Actuated Cycle Length: 140						STATE OF THE PARTY	
Offset: 0 (0%), Referenced		AIDTL	4 E-NIDI	Otant of C		22.00	

Natural Cycle: 80

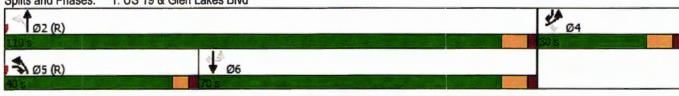
Control Type: Actuated-Coordinated

Maximum v/c Ratio: 0.81

Intersection Signal Delay: 27.5 Intersection Capacity Utilization 72.4% Intersection LOS: C
ICU Level of Service C

Analysis Period (min) 15

Splits and Phases: 1: US 19 & Glen Lakes Blvd



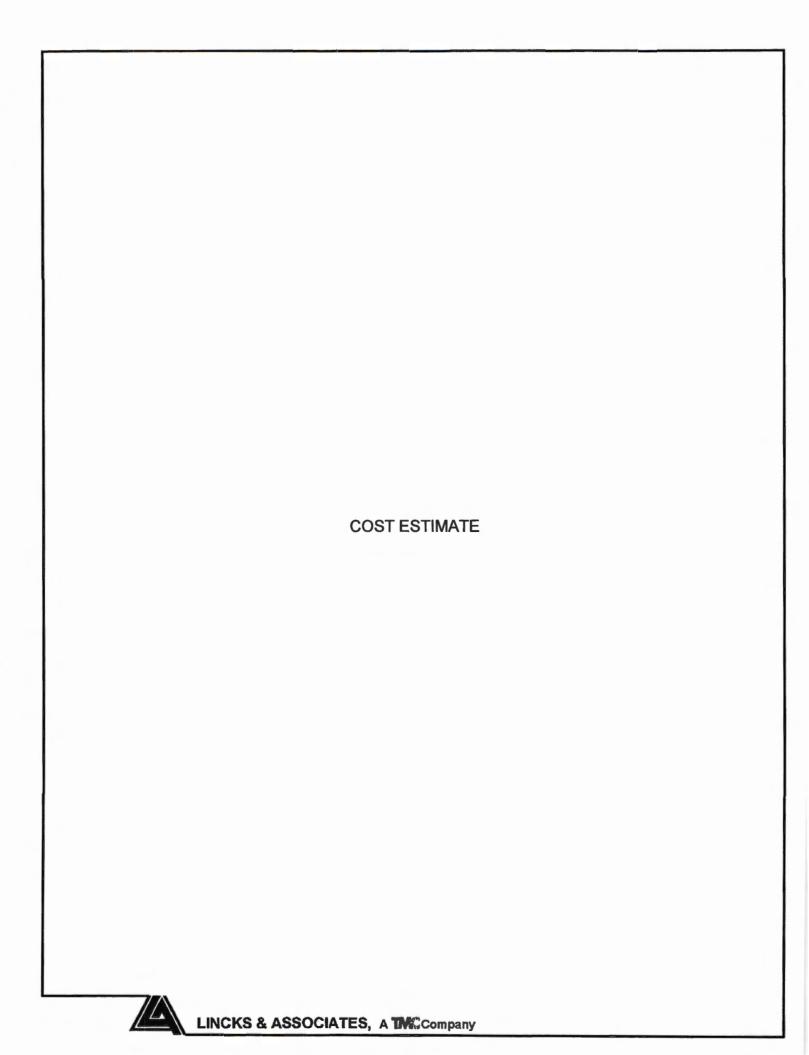
	1	-	1	†	L	ļ	1
Movement	EBL	EBR	NBL	NBT	SBU	SBT	SBR
Lane Configurations	*	7	7	^	Ð	^	7
Traffic Volume (vph)	149	172	305	890	12	1092	51
Future Volume (vph)	149	172	305	890	12	1092	51
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900
Total Lost time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Lane Util. Factor	1.00	1.00	1.00	0.95	1.00	0.95	1.00
Frt	1.00	0.85	1.00	1.00	1.00	1.00	0.85
Fit Protected	0.95	1.00	0.95	1.00	0.95	1.00	1.00
Satd. Flow (prot)	1770	1495	1736	3406	1770	3438	1495
Flt Permitted	0.95	1.00	0.09	1.00	0.28	1.00	1.00
Satd. Flow (perm)	1770	1495	157	3406	528	3438	1495
Peak-hour factor, PHF	0.88	0.88	0.88	0.88	0.88	0.88	0.88
Adj. Flow (vph)	169	195	347	1011	14	1241	58
RTOR Reduction (vph)	0	15	0	0	0	0	25
Lane Group Flow (vph)	169	180	347	1011	14	1241	33
Heavy Vehicles (%)	2%	8%	4%	6%	2%	5%	8%
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	pm+ov
Protected Phases	4	5	5	2	. 51111	6	4
Permitted Phases		4	2	Company of	6		6
Actuated Green, G (s)	18.1	57.0	106.9	106.9	62.5	62.5	80.6
Effective Green, g (s)	18.1	57.0	106.9	106.9	62.5	62.5	80.6
Actuated g/C Ratio	0.13	0.41	0.76	0.76	0.45	0.45	0.58
Clearance Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Lane Grp Cap (vph)	228	608	558	2600	235	1534	940
v/s Ratio Prot	c0.10	0.08	c0.17	0.30	200	c0.36	0.00
v/s Ratio Perm	00.10	0.04	0.30	0.00	0.03	00.00	0.02
v/c Ratio	0.74	0.30	0.62	0.39	0.06	0.81	0.02
Uniform Delay, d1	58.7	28.0	30.8	5.6	22.0	33.6	12.9
Progression Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00
Incremental Delay, d2	12.2	0.3	5.1	0.4	0.1	3.3	0.0
Delay (s)	70.9	28.3	36.0	6.0	22.1	36.8	12.9
Level of Service	70.9 E	20.3 C	30.0 D	Α.0	C C	30.0 D	12.9 B
Approach Delay (s)	48.1		U	13.7		35.6	D
Approach LOS	40.1 D			13.7 B		30.0 D	
	U			D		U	
Intersection Summary							
HCM 2000 Control Delay			27.3	Н	CM 2000	Level of	Service
HCM 2000 Volume to Capac	city ratio	V	0.74				
Actuated Cycle Length (s)			140.0		um of lost		
Intersection Capacity Utiliza	tion		72.4%	IC	U Level o	of Service	
Analysis Period (min)			15				
c Critical Lane Group							

	۶	*	1	†	L	+	1	
ane Group	EBL	EBR	NBL	NBT	SBU	SBT	SBR	
ane Configurations	M	7	7	*	Ð	**	7	
Traffic Volume (vph)	137	149	286	1214	33	1219	70	
uture Volume (vph)	137	149	286	1214	33	1219	70	
Peak Hour Factor	0.98	0.98	0.98	0.98	0.98	0.98	0.98	
Heavy Vehicles (%)	2%	9%	2%	3%	2%	5%	6%	
Shared Lane Traffic (%)								
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	Perm	
Protected Phases	4	5	5	2		6		
Permitted Phases		4	2		6		6	
Detector Phase	4	5	5	2	6	6	6	
Switch Phase								
Minimum Initial (s)	5.0	5.0	5.0	7.0	7.0	7.0	7.0	
Minimum Split (s)	12.5	10.5	10.5	14.5	14.5	14.5	14.5	
Total Split (s)	30.0	45.0	45.0	110.0	65.0	65.0	65.0	
Total Split (%)	21.4%	32.1%	32.1%	78.6%	46.4%	46.4%	46.4%	
Yellow Time (s)	5.5	3.5	3.5	5.5	5.5	5.5	5.5	
All-Red Time (s)	2.0	2.0	2.0	2.0	2.0	2.0	2.0	
Lost Time Adjust (s)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Total Lost Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5	
Lead/Lag		Lag	Lag		Lead	Lead	Lead	
Lead-Lag Optimize?		Yes	Yes		Yes	Yes	Yes	
Recall Mode	None	None	None	C-Min	C-Min	C-Min	C-Min	
Act Effct Green (s)	16.3	50.3	110.7	108.7	76.7	76.7	76.7	
Actuated g/C Ratio	0.12	0.36	0.79	0.78	0.55	0.55	0.55	
v/c Ratio	0.12	0.30	0.78	0.46	0.17	0.66	0.08	
Control Delay	75.3	27.3	27.9	6.4	22.9	26.4	4.7	
the state of the s	0.0	0.0	0.0	0.0	0.0	0.0	0.0	
Queue Delay			27.9	6.4	22.9	26.4	4.7	
Total Delay	75.3 E	27.3 C	21.9 C	0.4 A	22.9 C	20.4 C	4.1 A	
LOS		C	U	10.5	U	25.2	^	
Approach Delay	50.3					25.2 C		
Approach LOS	D			В		C		
ntersection Summary								
Cycle Length: 140								
Actuated Cycle Length: 140								
Offset: 0 (0%), Referenced to	to phase 2	:NBTL ar	d 6:SBTL	J, Start of	Green			
Natural Cycle: 65								
Control Type: Actuated-Coo	rdinated							
Maximum v/c Ratio: 0.68								
ntersection Signal Delay: 20	0.4			li	ntersectio	n LOS: C		
Intersection Capacity Utiliza		6		1	CU Level	of Service	e D	
Analysis Period (min) 15								
Splits and Phases: 1: US	19 & Glen	Lakes B	lvd					

\$ Ø5

Ø6 (R)

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Movement	EBL	EBR	NBL	NBT	SBU	SBT	SBR	
Lane Configurations	7	7	P.	^	Ð	个个	7	
Traffic Volume (vph)	137	149	286	1214	33	1219	70	
Future Volume (vph)	137	149	286	1214	33	1219	70	
Ideal Flow (vphpl)	1900	1900	1900	1900	1900	1900	1900	
Total Lost time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5	
Lane Util. Factor	1.00	1.00	1.00	0.95	1.00	0.95	1.00	
Frt	1.00	0.85	1.00	1.00	1.00	1.00	0.85	
FIt Protected	0.95	1.00	0.95	1.00	0.95	1.00	1.00	
Satd. Flow (prot)	1770	1482	1770	3505	1770	3438	1524	
Flt Permitted	0.95	1.00	0.15	1.00	0.20	1.00	1.00	
Satd. Flow (perm)	1770	1482	276	3505	364	3438	1524	
Peak-hour factor, PHF	0.98	0.98	0.98	0.98	0.98	0.98	0.98	
Adj. Flow (vph)	140	152	292	1239	34	1244	71	
RTOR Reduction (vph)	0	12	0	0	0	0	32	
Lane Group Flow (vph)	140	140	292	1239	34	1244	39	
Heavy Vehicles (%)	2%	9%	2%	3%	2%	5%	6%	
Turn Type	Prot	pm+ov	pm+pt	NA	Perm	NA	Perm	
Protected Phases	4	5	5	2		6		NAME OF TAXABLE PARTY.
Permitted Phases		4	2	and the second	6		6	
Actuated Green, G (s)	16.3	42.8	110.7	108.7	76.7	76.7	76.7	
Effective Green, g (s)	16.3	42.8	110.7	108.7	76.7	76.7	76.7	
Actuated g/C Ratio	0.12	0.31	0.79	0.78	0.55	0.55	0.55	Marie Terres de la Vide de la Vid
Clearance Time (s)	7.5	5.5	5.5	7.5	7.5	7.5	7.5	
Vehicle Extension (s)	3.0	3.0	3.0	3.0	3.0	3.0	3.0	
ane Grp Cap (vph)	206	511	501	2721	199	1883	834	
//s Ratio Prot	c0.08	0.05	c0.11	0.35	100	c0.36		
v/s Ratio Perm	00.00	0.04	0.35	0.00	0.09	00.00	0.03	
v/c Ratio	0.68	0.27	0.58	0.46	0.17	0.66	0.05	
Uniform Delay, d1	59.3	36.8	24.5	5.4	15.8	22.4	14.7	
Progression Factor	1.00	1.00	1.00	1.00	1.00	1.00	1.00	
Incremental Delay, d2	8.6	0.3	1.7	0.6	1.9	1.8	0.1	
Delay (s)	68.0	37.1	26.2	6.0	17.6	24.3	14.8	
Level of Service	E	D	C	Α	17.0 B	24.5 C	В	
Approach Delay (s)	51.9	5		9.8		23.6		
Approach LOS	D			Α		23.0 C		
Intersection Summary	'STREET						State of the last	
HCM 2000 Control Delay		-	19.6	Н	CM 2000	l evel of	Service	В
HCM 2000 Volume to Capac	city ratio		0.65		CHI 2000	LOTOI OI V	OCI VIOC	
Actuated Cycle Length (s)	, .uu		140.0	S	um of lost	time (e)		20.5
ntersection Capacity Utilizat	tion		74.2%		U Level o			20.5 D
Analysis Period (min)		A Delivery and the	15	10	Level	OGI VICE	A STATE OF THE PARTY	U
Critical Lane Group		SALES OF THE PARTY	10				Marine Land Commen	



ENGINEER'S CONSTRUCTION COST ESTIMATE

SIGNALIZATION PROJECT

U.S. 19 @ GLEN LAKES BOULEVARD HERNANDO COUNTY, FLORIDA

Estimated By: Company: Date: Project No.:	Robert W. Fulp, PE (Engineer-of-Record) Lincks & Associates, Inc. 7-Feb-25 22050 / F4			
Roadway	\$ 171,012			
Signing / Pavement Markings Signalization & Lighting	\$ 15,000 \$ 796,025			
Sub-Total	\$ 982,037			
Contingency Items (10%)	\$ 98,204	(3)		
Maintenance of Traffic (5%)	\$ 49,102			
Mobilization (5%)	\$ 49,102			
Bonding (3%)	\$ 29,461			
MOT Plans	\$ 3,500	(2)		
Construction Surveying	\$ 5,000	(2)		
Permits	\$ 2,000	(2)		
Material Testing	\$ 5,000	(2)		
As-Builts	\$ 3,500	(2)		
Sub-Total	\$ 1,226,906			
Independent Contractors Fee (8%)	\$ 98,152			
Grand Total	\$ 1,325,058			

Notes:

- (1) The estimate was based on FDOT's current 12-month (12/1/23 11/31/24) moving average unit costs for Area 7, which includes Hernando County. See attached plans.
- (2) This is an estimated value based on the size of the project.
- (3) Due to project unknowns, such are unexpected utility relocations, etc., a continency of 10% was assumed.

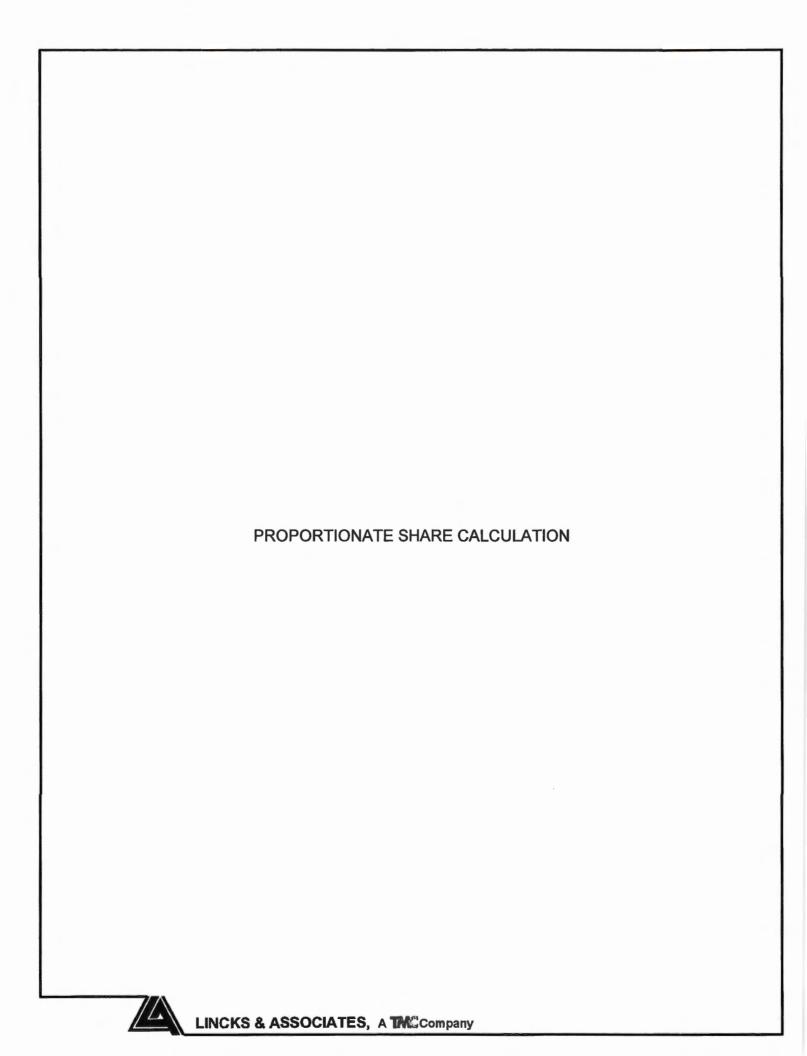




TABLE A-1
PROPORTIONATE SHARE CALCULATION

<u>Intersection</u>	Time Period	Critical Movement	LOS E Lane Group <u>Capacity</u>	LOS D Lane Group Capacity (1)	New Project Trips
US 19 and Glen Lakes Blvd	АМ	EB Left NB Left SB Through	228 558 1,534	202 494 <u>1,358</u> 2,054 % Consumed	119 248 <u>174</u> 541 26.34%
	PM	EB Left NB Left SB Through	206 501 1,883	182 443 <u>1,666</u> 2,291 % Consumed	117 199 <u>134</u> 450 19.64%

(1) Adjusted by 0.885